

City Centre  
Development  
Corporation


City of Las Vegas  
Planning &  
Development  
Department

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April 27, 2000

CCDC Adopted:  
February 17, 2000



# LAS VEGAS DOWNTOWN CENTENNIAL plan

ESTABLISHING FUNDAMENTAL PLANNING,  
DEVELOPMENT, LAND USE CONCEPTS,  
AND URBAN DESIGN STANDARDS TO  
GUIDE REDEVELOPMENT THROUGH THE  
CENTENNIAL AND BEYOND



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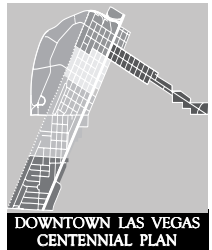
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**DOWNTOWN LAS VEGAS  
CENTENNIAL PLAN**



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*\* All maps oriented North*

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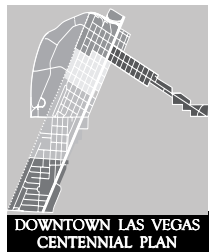
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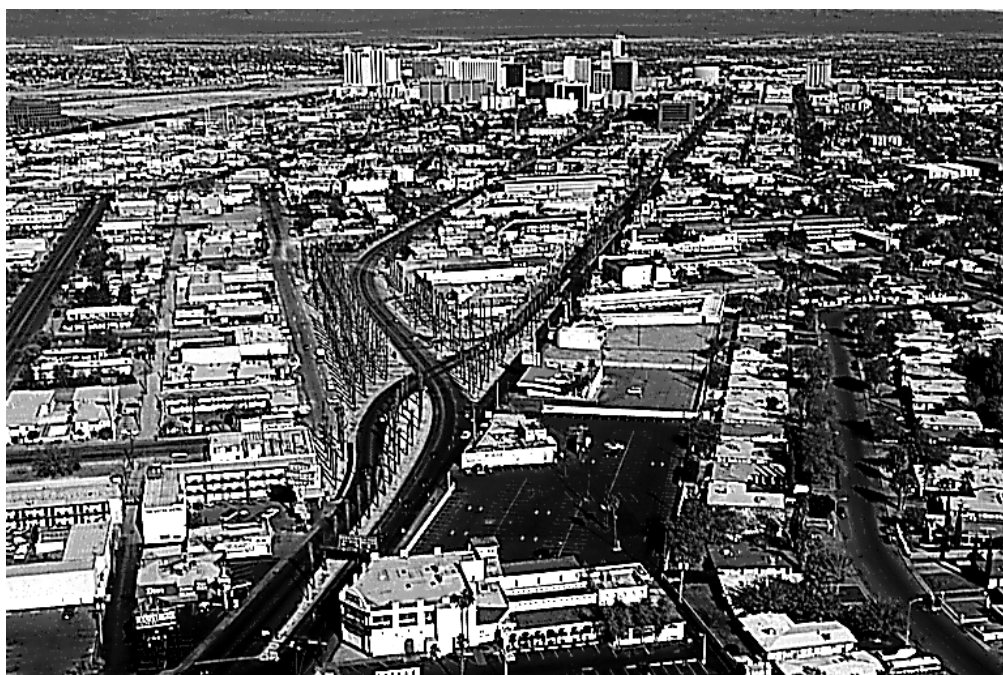


## V. VISION OF DOWNTOWN LAS VEGAS CENTENNIAL

1 Las Vegas will celebrate the 100th anniversary of its founding in 2005 and its incorporation as a City in 2011. The Downtown Las Vegas Centennial Plan (hereinafter “the Plan”) establishes fundamental planning, development, and land use concepts, and especially urban design standards to guide the redevelopment of the original core of the burgeoning Las Vegas Valley through its centennial and beyond. This plan is a component of the City’s new 2020 Master Plan, and therefore, shares a 20 year horizon for full implementation. Several major projects are currently in development that will begin to change the face, and the public’s impressions, of downtown. These projects will greatly improve the tax base for the City—which will in turn enhance future redevelopment opportunities. Jobs will be created— construction and permanent, professional and service employment. The critical mass of several thousand new workers and some 12,000 additional daily visitors to downtown will sustain a variety of new



PHOTO 1: AERIAL OF DOWNTOWN LAS VEGAS



cafes, restaurants, and supporting commercial uses. Within three to five years, tremendous opportunities for new development will emerge in and around downtown, from high-rise condominiums with premium valley-wide views to cultural centers. This growing synergy of uses will create a dynamic, rich, urbane environment that will promote other redevelopment and desirable land uses downtown. This will create a complex and intriguing pedestrian-oriented 24-hour lifestyle.

**2** This Plan sets forth a long-term strategy to re-establish Downtown Las Vegas as the region's premier cultural, civic, financial, and business center. Downtown is becoming a destination that tourists, residents, and business people will seek out for its employment and residential opportunities, ease of access, and unique image. It will have high quality office complexes, restaurants catering to both business and evening activity patrons, parks, plazas, streets lined with trees and arcades, hotels, the "historic" casinos, entertainment, shopping, museums, and perhaps most importantly, new residential communities and neighborhoods. It remains the home to Las Vegas City Hall, the Clark County Government Seat, regional courts, federal and civic uses. The resurgence of activity will generate economic vitality and make Downtown Las Vegas a safe and pleasant place to live, work, and play.



**3** The Plan's vision section provides details on all the new development projects springing up downtown, describes the history of downtown, defines the area of this particular study, and explains the history of previous planning efforts that have led up to this new Plan.

## V1. DOWNTOWN RENAISSANCE

**1** Downtown Las Vegas is beginning its first renaissance as the true urban center for the entire Las Vegas Valley. The opening of the Fremont Street Experience in 1995 (Photo 2) marked the beginning of a series of new development projects and a succession of rapid, positive changes. The new Clark County Government Center (Photo 3) was dedicated in 1997, anchoring the initial redevelopment of the old Union Pacific rail yards and setting high architectural standards for all development to follow.

**2** The City of Las Vegas has invested some 25 million dollars in public infrastructure between 1996 and 1998. The Fourth Street Corridor (Photo 4), lined with palm trees

PHOTO 2: FREMONT STREET EXPERIENCE

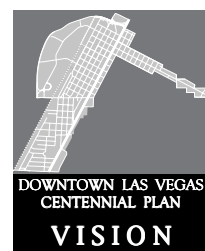
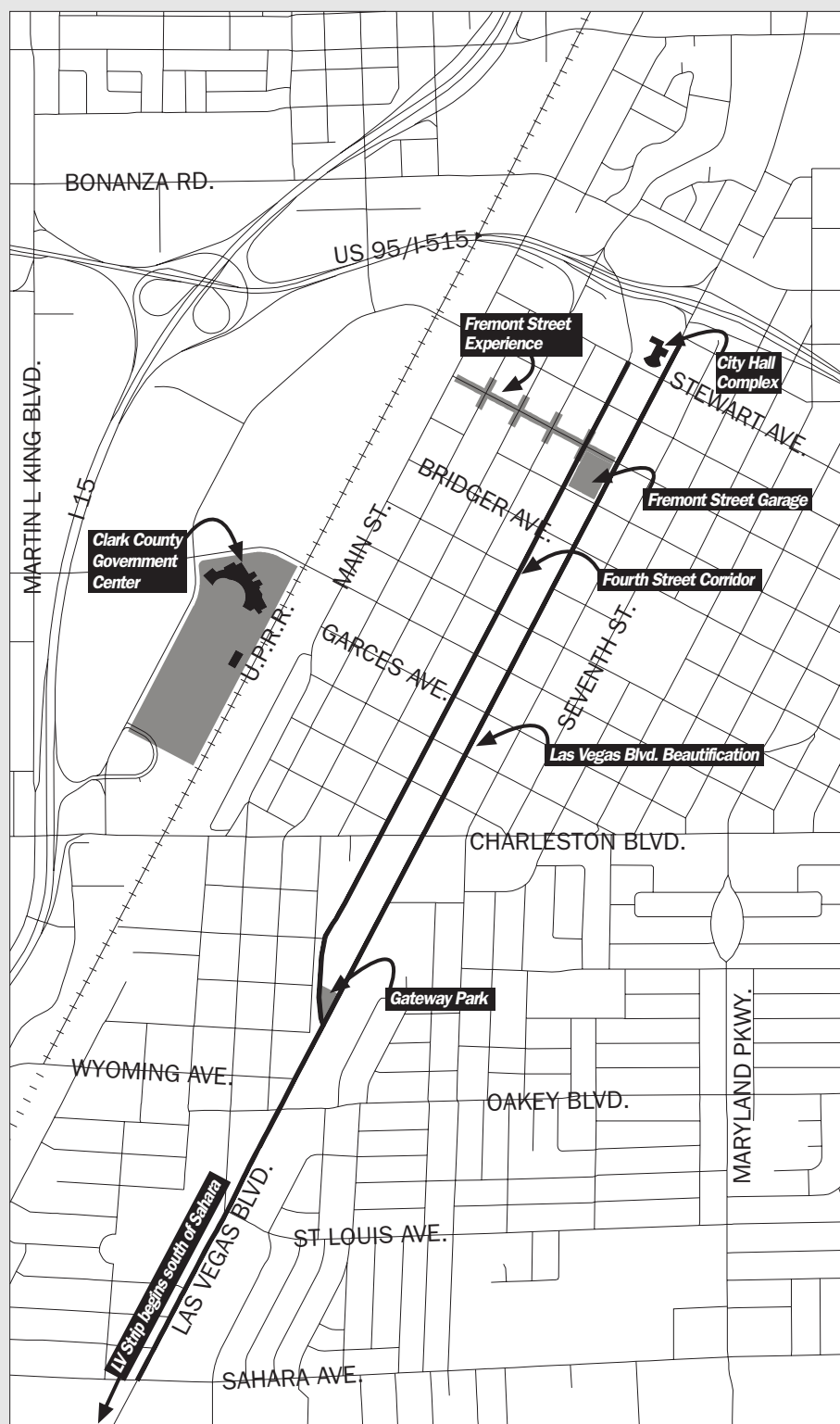


PHOTO 3: CLARK COUNTY GOVERNMENT CENTER



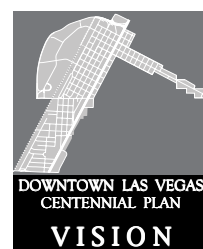
PHOTO 4: FOURTH STREET CORRIDOR IMPROVEMENTS





— Recent Streetscape Improvements

■ Recent Development Projects

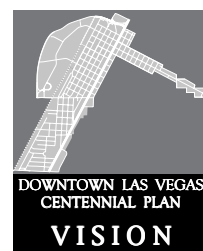


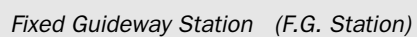
MAP 1: DOWNTOWN: RECENT DEVELOPMENTS



and wide sidewalks, has been completely rebuilt from Las Vegas Boulevard to Interstate 515. Gateway Park (Photo 5), with its lush canopy of 222 palm trees, marks the arrival of downtown and anchors the southern realms of downtown. Las Vegas Boulevard has been beautified with a landscaped median from one end of downtown to the other, providing visitors with seamless visual continuity from the Strip to City Hall (Map 1). The City continues to plan for the future and will support private development with additional right-of-way and streetscape improvements. Two additional projects are now being discussed: the Lewis Street Corridor which will create the central spine for the Office Core District, and the Third Street Pedestrian Connector which will greatly enhance foot traffic between the Fremont Street Experience and the Lewis Street Corridor and later southward into new residential neighborhoods and the Arts District (Map 2).

PHOTO 5: GATEWAY PARK





MAP 2: DOWNTOWN: CURRENT DEVELOPMENTS

3 Currently, almost one billion dollars (\$1,000,000,000.) is being invested in construction projects in Downtown Las Vegas (Map 2):

- a. Federal Building / United States Courthouse broke ground in April of 1998 and is scheduled for completion in the Spring of 2000. It will contain some 407,000 square feet of federal court rooms and offices and represents the investment of more than 90 million dollars in federal funds (Graphic 1).
- b. Intown Office, The Intown Office, L.L.C., with the Pauls Corporation of Denver, is developing a 110,000 square foot Class A office building downtown in cooperation with City Center Development Corporation and the Downtown Redevelopment Agency. The building will be located at Fourth and Lewis, between the Regional Justice Center and the Fifth Street School complex. The Pauls tower will be integrated with a public parking garage and the City's improvement project on Lewis Street. Ground floor retail and commercial will contribute vitality to the Lewis Street project's pedestrian and landscape beautification (Graphic 2).



GRAPHIC 1: FEDERAL BUILDING / U.S. COURTHOUSE



Rendering by HCA / Dworsky Architects

GRAPHIC 2: INTOWN OFFICE



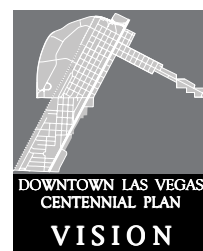
Rendering by Chris Hylton

GRAPHIC 3: REGIONAL JUSTICE CENTER



Rendering by Tate &amp; Snyder Architects

- c. Clark County Regional Justice Center complex will occupy two urban blocks and marks an unprecedented cooperative arrangement between the City of Las Vegas and Clark County. This 18-story tower is scheduled for completion in summer 2002 and will contain 700,000 square feet of courts, public, and support space, including the District Attorney's offices. This 132 million dollar investment includes the City's commitment of 32 million dollars to relocate its Municipal Courts (Graphic 3).
- d. Clark County Detention Center Expansion is underway at a cost of 78 million dollars with an opening set for December 2000.
- e. Neonopolis, an urban entertainment destination center, will anchor the Las Vegas Boulevard end of the Fremont Street Experience and provide an exciting venue along Glitter Gulch for shopping, dining and catching a movie. This 100 million dollar project, scheduled to open in late 2001, is a joint venture of the City and World Entertainment



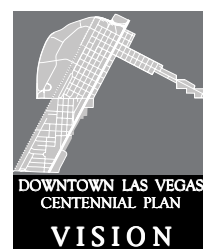
GRAPHIC 4: NEONOPOLIS RENDERING



Rendering by RTKL Architects

Centers / Las Vegas, Inc. (Prudential Insurance Company of America). The City has assisted with land acquisition and will build and retain ownership of a 600 space subterranean public parking garage (Graphic 4).

- f. Resort Corridor Fixed Guideway System, Phase One, will provide public transportation linking downtown to Cashman Field and the Las Vegas Strip. This initial 380 million dollar project of the Regional Transportation Commission is partially funded by 155 million dollars in federal funds and will likely open by 2005. The second phase may complete a loop system around the Strip and on to McCarran International Airport. Future phases could link suburban areas such as Town Center, Summerlin, and Green Valley to downtown (Map 10).
- g. City Hall Campus expansion and renovation is proposed in four phases over the next 20 years and represents a commitment of \$43 million by the City of Las Vegas.
- h. Historic Fifth Street School re-use scenarios are currently being explored by the City and City Centre Development Corporation and could include community and cultural uses, a festival marketplace, and other supportive uses such as housing.
- i. Downtown Beautification Projects by the City total more than 25 million dollars.



**4** These projects will create a new critical mass of people which will create further market demands for additional cafes, retail, and entertainment venues, thus transforming Downtown Las Vegas into a sophisticated and urbane environment. The redevelopment of the Arts Factory building (Photo 6) and the creation of other galleries and cafes (Photo 7) in the Downtown South District have made it feasible to support the concept of creating an Arts District downtown with live-work and smaller-scaled redevelopment opportunities.

**5** Within the next few years, the development of new residential communities will not only be possible, but highly desirable in rounding out the land uses downtown while creating an around-the-clock atmosphere. New residential towers with panoramic valley views, townhouses, and courtyard apartments will begin to rise on under-utilized parcels, and live-work lofts will be converted out of older office structures. Within the next few years, the fate of the vacant railyards may be determined with new buildings rising on this now vacant site readied for development.

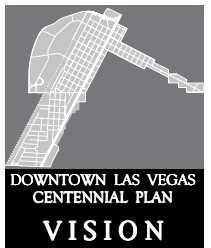


PHOTO 6: ARTS FACTORY BUILDING, A COLLECTION OF STUDIOS AND GALLERIES IN THE HEART OF THE ARTS DISTRICT

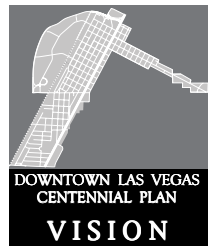


PHOTO 7: ENIGMA CAFE, ALSO IN THE ARTS DISTRICT



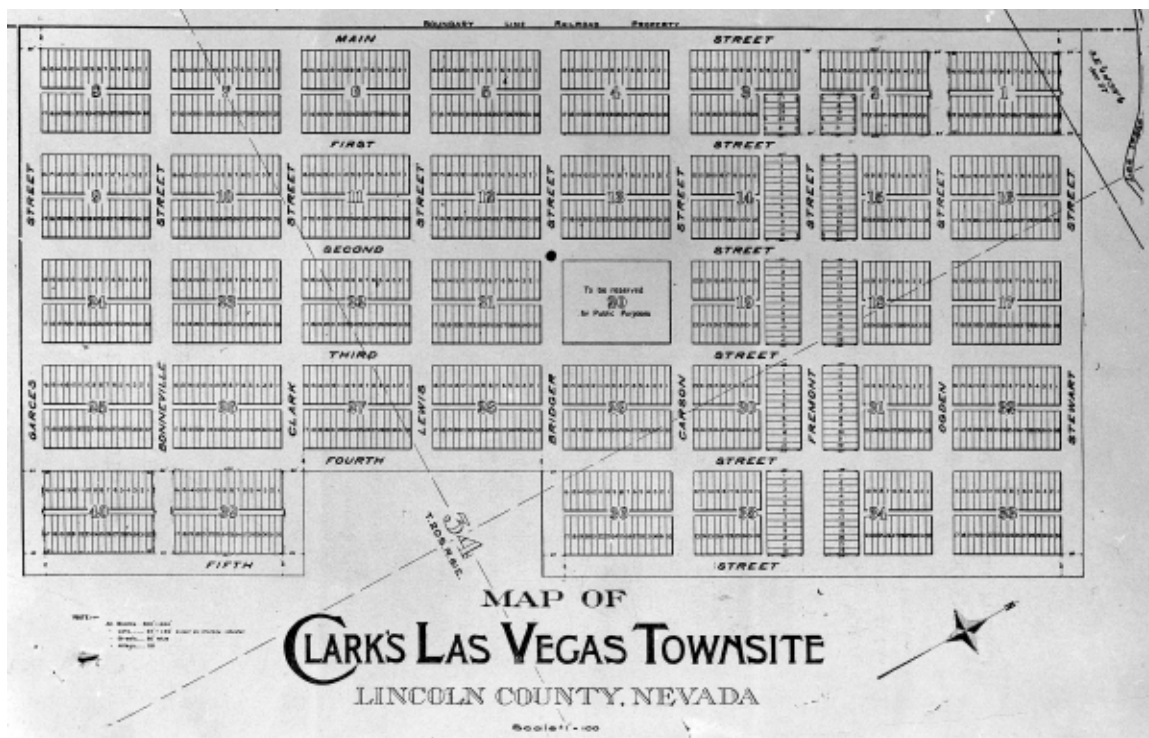


1 Las Vegas was founded by the Union-Pacific Railroad on May 5, 1905, as a commercial real estate venture. The railroad's realtor laid out an original grid of 40 blocks, each 300 by 400 feet, long-wise along the train tracks, and sold off lots on those blocks at auction. A 20 foot wide alley broke most blocks in half, and the halves were generally sub-divided into 16, 25-foot lots, each 14 feet deep (32 lots to the block maximum) (Graphic 5). The railroad station sat proudly in a park on the west side of Main Street and at the head of Fremont Street (the site of today's Union Plaza Hotel / Casino). The railroad's employees were the first occupants of the new town. Fremont Street was parceled up to be the main commercial street, running east-west against the grid. North-south streets ran parallel to Main and number First, Second, Third, and so on moving eastward. However, Fifth Street has since become Las Vegas Boulevard,

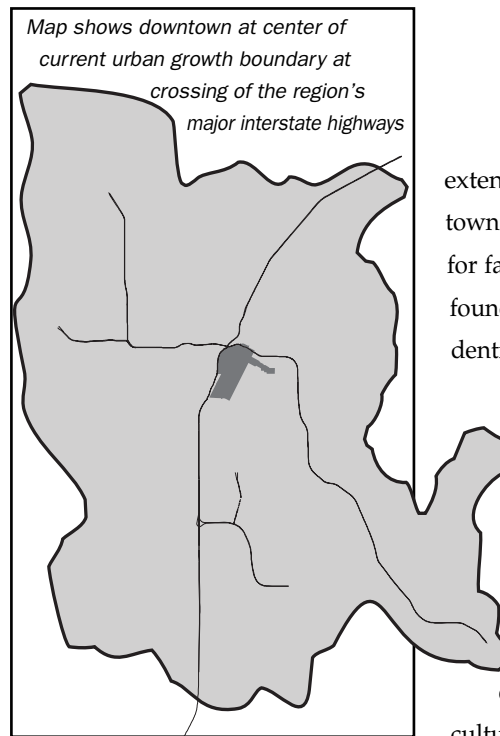


MAP 3: DOWNTOWN: HISTORIC BLOCK AND PARCEL PATTERN

*Las Vegas was founded by the Union-Pacific Railroad on May 5, 1905, as a commercial real estate venture. The railroad's realtor laid out an original grid of 40 blocks, each 300 by 400 feet, long-wise along the train tracks, and at auction sold off lots on those blocks. A 20-foot wide alley broke most blocks in half, and the halves were generally sub-divided into 16, 25 foot lots, each 140 feet deep (32 lots to the block maximum).*

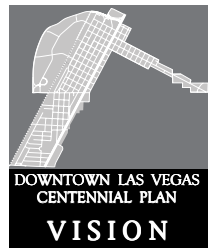


MAP 4: DOWNTOWN AS THE HUB OF THE LAS VEGAS VALLEY

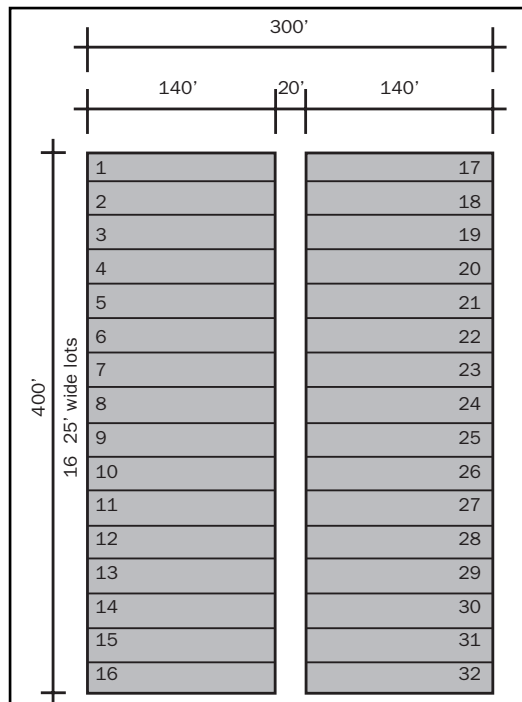


extending “The Strip” northward into Downtown Las Vegas. East-west streets were named for famous folks who were influential in the founding of Nevada (Map 3). A mix of residential and commercial uses, including gambling halls and saloons, sprung up in this gridded, dusty desert. The City was incorporated under State of Nevada Statutes in 1911.

**2** Downtown Las Vegas has a rich legacy of business, commerce, entertainment, lodging, and community culture. It ranges from the founding of the First State Bank of Las Vegas on the corner of First and Fremont Streets in 1905 to the ultra, high-tech entertainment spectacular of today’s Fremont Street Experience. Downtown Las Vegas has long served as the hub of the entire Las Vegas region (Map 4). Over the last generation though, office development has dispersed to new suburban centers like Summerlin, Green Valley, Hughes Center, and McCarran Airport Center. New housing development has spread out across the valley. Downtown Las Vegas, once the residential, financial, and cultural heart of the City, has been regularly passed over for the allure of new suburban oases.



GRAPHIC 5: ORIGINAL DOWNTOWN BLOCK PATTERN AND SMALL PARCEL LAYOUT



**3** The original block pattern, and especially the small parcel layout, presents the greatest challenge today for downtown redevelopment (Graphic 5). Many 25 and 50 foot wide lots still remain in the Office



Core District and across downtown, under separate ownership. Modern development seldom or never happens in 25 foot wide increments. The smallest parcel feasible for current development is larger than a half block, with many on a full block or larger. Also, the entire Office Core area and most of downtown were removed from the Gaming Enterprise Overlay District by City Council in 1996, and the Nevada State Legislative in 1997, to relieve the artificially high land values. This will work to concentrate casino activity along the Fremont Street Experience area.

PHOTO 8: FREMONT STREET IN 1911, AT TIME OF INCORPORATION

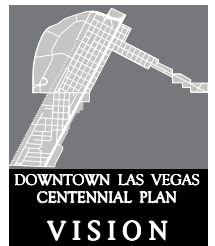


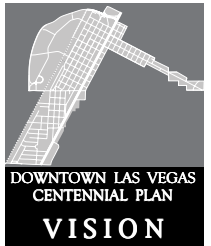
PHOTO 9: FREMONT STREET IN 1958



## V2. POLICY CONTEXT FOR DOWNTOWN LAS VEGAS

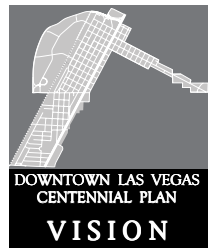
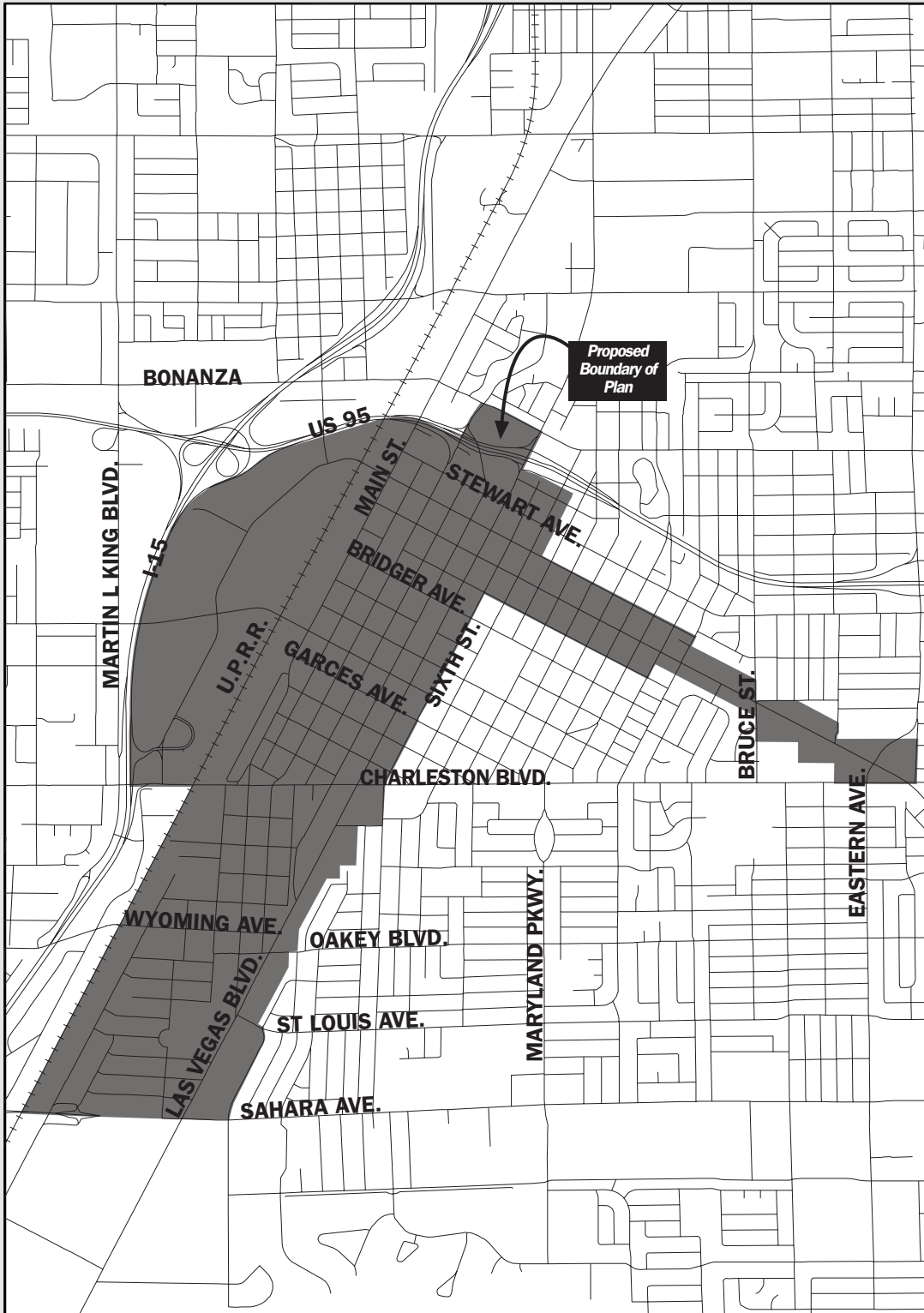
### V2A. AREA OF STUDY

**1** Within the context of this Plan, Downtown Las Vegas shall be defined to mean the geographic area generally contained within the boundaries of the northern parcels along Bonanza Road on the north, eastern parcels along Sixth Street on the east, Sahara Avenue on the south and Interstate-15 on the west, and extending along East Fremont Street to Charleston Boulevard. The centerline of Sahara Avenue is the boundary between the City of Las Vegas and unincorporated Clark County. This land area, approximately one-half mile wide and one and one-half miles long, is divided into seven unique districts. This area contains nearly 1600 acres (Map 5).

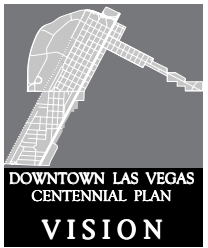
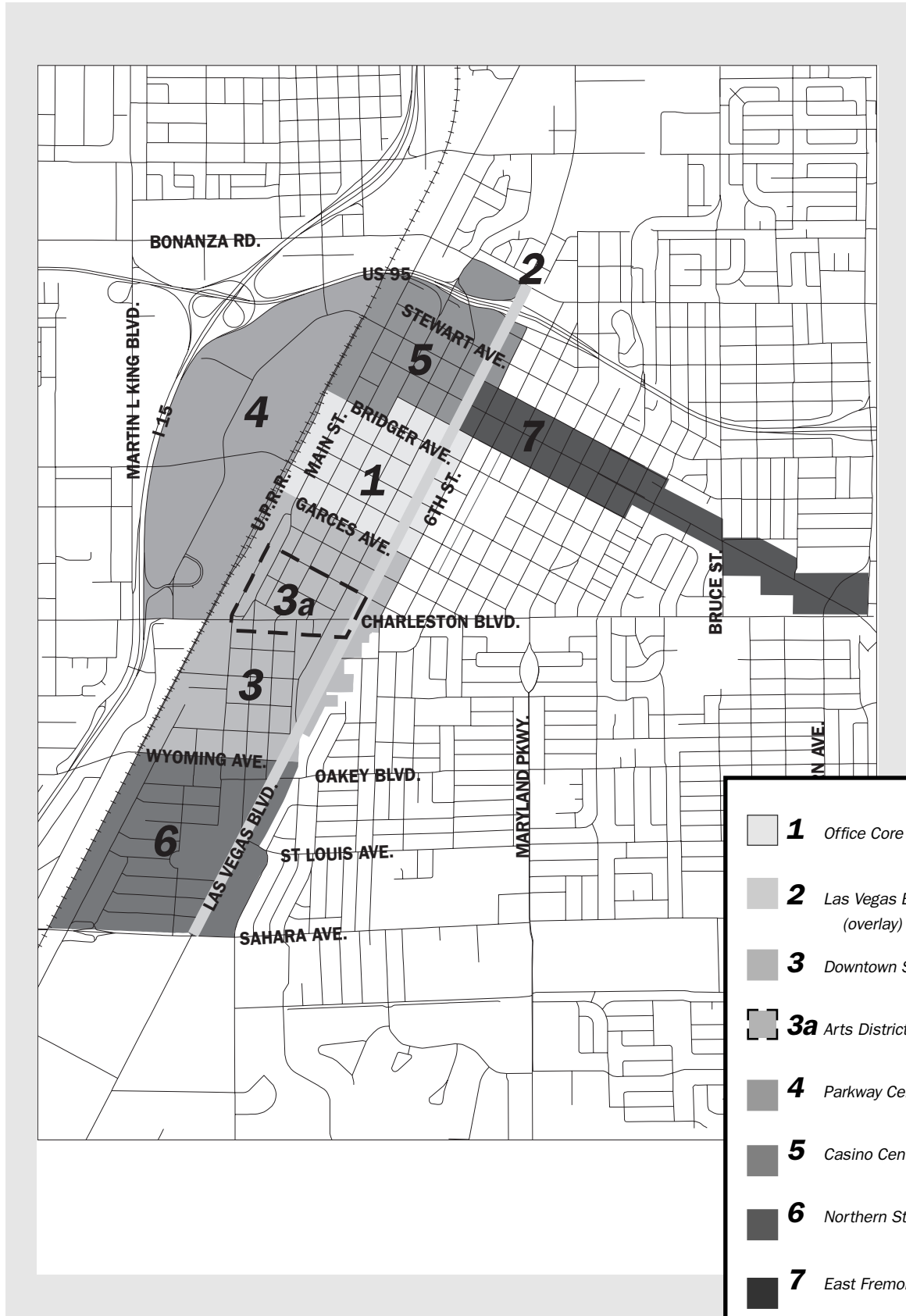


**2** This area of study is the major, central portion of the City of Las Vegas Downtown Redevelopment Area, but excludes the Redevelopment Agency's surrounding local neighborhood centers which are not a part of this plan. In June 1998, City Centre Development Corporation (CCDC), in agreement with the City Council sitting as the Agency, expanded its area of influence to include much of this area of study.

**3** The area of study is conceptually divided into seven planning districts (Map 6). The boundary and character of each district is defined by the predominant type and intensity of land uses, general architectural character, and qualities of the streetscape within that area. All the districts are linked together by Las Vegas Boulevard commonly called "The Strip" to the south or "The Boulevard" as it passes through the City and beyond its limits to the north.



MAP 5: LAS VEGAS DOWNTOWN CENTENNIAL PLAN BOUNDARY



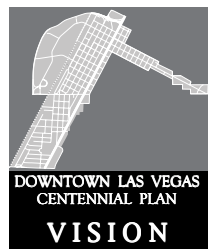
- 1** Office Core
- 2** Las Vegas Blvd. (overlay)
- 3** Downtown South
- 3a** Arts District
- 4** Parkway Center
- 5** Casino Center
- 6** Northern Strip
- 7** East Fremont

MAP 6: SEVEN DOWNTOWN PLANNING DISTRICTS

**1** Efforts to competitively reposition Downtown Las Vegas began in April 1986 when the City of Las Vegas Downtown Redevelopment Agency (DRA) engaged the consulting firm of Laventhol and Horwath, CPA, of Los Angeles, to prepare a development analysis and strategy for Downtown Las Vegas. The policies in that plan and strategy enabled the DRA to react to developer proposals as they were presented. This strategy predominated until 1993 when the DRA Board of Directors sought to establish a more proactive stance to the redevelopment efforts of Downtown Las Vegas. The firm of Trkla, Pettigrew, Allen & Payne, Inc. (TPAP) was engaged to update the agency's comprehensive plan and redevelopment strategy.

**2** A summary of the primary observations of the 1993 TPAP study include:

- a. Existing conditions do not contribute to significant redevelopment goals. Downtown Las Vegas is the original Office Core, but does not have office uses typically found in most Central Business Districts. Most tenants are government offices and related uses. Many old, single-family homes have been converted to offices that primarily house attorneys and accountants. "Prestige" addresses are not found downtown, but are located in remote, sub-market areas like the Hughes Center, Summerlin, Green Valley and office parks around McCarran Airport.
- b. Downtown Las Vegas redevelopment efforts will increase a positive "critical mass" of diverse but complimentary uses, halting the trend of "fractionalized" office activity.
- c. Seven planning areas were identified as sub-components of Downtown, each with its unique characteristics, opportunities, and constraints.
- d. Casino Center, the historic concentration of casinos lining Fremont Street and its adjacent streets, will conserve and enhance the pedestrian character with tourist-serving uses including hotels, casinos, specialty retail, restaurants, and entertainment attractions.
- e. Entryway design improvements into Downtown should be implemented at key gateways and should include special lighting, signage, landscaping, and other features.
- f. The Office Core will promote the return of Downtown Las Vegas as the region's legal, governmental, and financial center. Office uses north of Garces Avenue will reinforce the image of the major governmental, business, and financial center while the blocks south of Garces should be mixed-use office, residential, and commercial uses woven into the urban fabric. Ground floor retail will be encouraged on the blocks north of

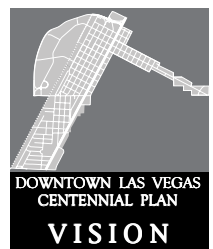


Bonneville. Low-intensity office uses with high-to-medium density residential will dominate the urban character south of Bonneville.

- g. The City should work with the Clark County Assessor to determine the appropriate and fair assessments of parcels and improvements within the downtown area, increasing assessments to at least 90 percent of current market value, or comparable. This will encourage divestiture of under utilized parcels and assist with assembly of small lots into useful redevelopment sites.
- h. Las Vegas Boulevard should become an attractive “identity street” as it passes through the Downtown Las Vegas area. It should consist of distinctive lighting, landscaping, and signage with its own urban design standards establishing the character and restrictions of development to occur within its edges. New development along the Boulevard should include commercial uses such as specialty retail, hotels, entertainment, and commercial services. The uses should be differentiated from similar types found in Casino Center and the Office Core, allowing them to retain their low-rise, small-scale character for commercial, governmental and institutional uses.

**3** Building upon the urban planning goals and framework provided in the 1993 TPAP study, the Denver office of the urban planning / design firm, EDAW, Inc. (in consultation with the market research firm, Growth Management, Inc., real estate development advisors, Keyser Marston Associates, Inc., and transportation consultant, TDA, Inc.) drafted an urban design master plan for the City of Las Vegas, City Manager’s Office. EDAW’s draft plan was adopted by Resolution of the City Council on October 2, 1996, as “draft design guidelines” only. The EDAW plan was intended to translate the TPAP urban planning and framework goals into a plan for future improvements, and to establish appropriate land use configurations in conjunction with design standards for image, character, landscaping, building massing, pedestrian systems, and public open space.

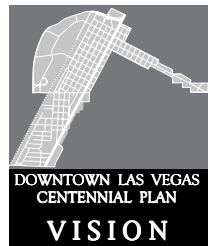
**4** The CCDC was formed by the City Council in late 1997 to oversee redevelopment activities in Downtown Las Vegas. City of Las Vegas Planning and Development Department staff, with the advice and consent of the CCDC Downtown Implementation Sub-Committee, edited, redrafted, and completed the Plan on February 2, 2000. The Plan was adopted by a resolution of the CCDC Board of Directors on February 17, 2000. CCDC recommended adoption of the plan to the Planning Commission and City Council, and it was adopted by Ordinance 2000 - 49 on July 5, 2000, as a General Plan Amendment.



**1** The Downtown Las Vegas Centennial Plan conforms to the General Plan of the City of Las Vegas, and is hereby incorporated into the City's 2020 Master Plan by way of Ordinance 2000-49, adopted July 5, 2000. The Downtown Las Vegas Centennial Plan supersedes and replaces the Downtown Urban Design Master Plan as adopted by Resolution as "Design Guidelines" by City Council on October 2, 1996.

**2** The Downtown Las Vegas Centennial Plan complements and coordinates with the new Downtown Overlay District, Title 19A.06.060 of the City of Las Vegas Zoning Code, as adopted March 1997 and amended thereafter, and is incorporated therein by Ordinance 2000-49 adopted July 5, 2000.

**3** The Downtown Las Vegas 2005 Plan complements and coordinates with the Redevelopment Plan for the Downtown Las Vegas Redevelopment Area, as adopted March 5, 1980, and amended thereafter.

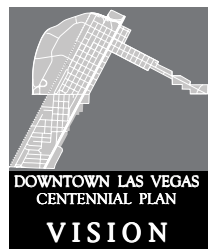


### V3. URBAN DESIGN FRAMEWORK

**1** The Urban Design Framework establishes the following urban design concepts and general urban planning principles for Downtown Las Vegas.

#### V3A. GATEWAYS

**1** Entry points to important places are largely ceremonial in nature, symbolically marking the point of passage from one district into another. Las Vegas has several entry points into the downtown area, many of which are not now distinctively delineated. The gateways into downtown should receive special treatment, including landscaping, lighting, signage, and other related improvements. This will make them distinctive within the urban environment and establish a civic presence and sense of pride for visitors, workers, and residents of downtown. Secondary gateways between the Office Core District and surrounding districts and neighborhoods should also feature physical improvements of landscaping, lighting, and signage to announce that transition.





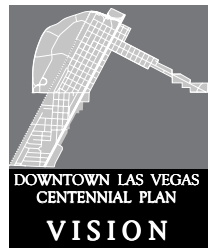
1 Las Vegas is world renown for its casinos and entertainment. This Plan builds upon that status in its delineation of substantial casino districts, perpetuating an image, character, and history that is uniquely Las Vegas. In downtown, the “historic” Casino Center District along Fremont Street to the north and the northern Strip casino district around the Stratosphere Tower to the south form two distinct areas of landmark status. The visual prominence of the Stratosphere Tower as the compass point for the Las Vegas Valley and the classic Las Vegas casinos nestled into the urban fabric along Fremont Street Experience provide downtown with two entertainment and activity anchors.



PHOTO 10 FREMONT STREET EXPERIENCE PLAZA: ENTERTAINMENT ANCHOR



**1** Las Vegas Boulevard is the downtown identity street. The Boulevard south of downtown is simply known as “The Strip.” Lined with casinos and the characteristic brilliant signage, it has established Las Vegas’ image as no other urban element. This Plan utilizes that imagery by extending the landscape continuity north into Downtown Las Vegas and building upon the legacy of The Strip, thereby forging a new identity for downtown. Las Vegas Boulevard forms the major north-south spine downtown. Through landscape improvements, right-of-way width expansion, lighting, signage, and urban design standards, it will become a key urban design and orientation element for downtown.

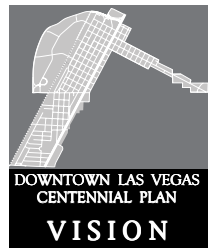


GRAPHIC 6: PROPOSED LAS VEGAS BLVD. LANDSCAPE IMPROVEMENTS, CREATING A “SPINE”



**1** Major vehicular linkages connect important places in the City, including anchors (like the casino districts), landmarks, neighborhoods, public uses, and private development. A system of linkages bolsters the integration of uses and overall design character. The City, with the Regional Transportation Commission (RTC), continues to widen and improve roadways not only entering and exiting downtown but throughout the entire Las Vegas valley.

**2** The RTC's proposed fixed guideway public transit system is moving forward with partial federal funding, with the first phase of construction to be up and running by 2005. Recent discussions suggest that the first phase of the Transrapid High-Speed Train, which will ultimately connect Downtown Las Vegas to Los Angeles and Anaheim, California, could well move ahead in the next decade with federal support for at least the first phase connecting Las Vegas to Primm, Nevada (at Stateline). The development of both systems, in addition to the restoration of Amtrak service in 2001, the continuation of Greyhound bus service, and the eventual need to expand the CAT bus system and link it to multiple rail lines, strongly suggest the future need and opportunity to develop a "Downtown Hub", or Intermodal Transportation Center. Such a center could serve as an obvious linkage between downtown and the old Union Pacific railyards site (called Parkway Center within the context of this Plan), and include substantial parking, retail and commercial facilities.



**3** Paralleling Las Vegas Boulevard, Fourth Street serves as a primary north-south linkage, enabling one-way vehicular traffic northbound with accompanying special treatments for the pedestrian realm, including unique paving, landscaping, and lighting elements. It serves as an alternative entryway and thoroughfare for downtown, relieving traffic congestion on Las Vegas Boulevard and connecting directly to interstate entry ramps. Careful consideration should be given to completing this one-way couplet by rebuilding Casino Center into a southbound one-way route from approximately Clark Street southward.

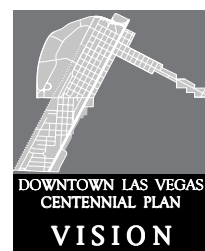
**4** Lewis Street should be developed into the primary east-west pedestrian corridor for the revitalized Office Core District. The intense concentration of new public and private developments along this street warrants the City's commitment to rebuild this street into the Lewis Street Corridor, or as some are already calling it, the new Civic Corridor because it links the Clark County Regional Justice Center and expanded jail facilities with the new Federal Building / U.S. Courthouse. The Intown Office Building

and the possible public-private re-use and revitalization of the historic Fifth Street School complex present enormous opportunities to again center business downtown. This critical mass of buildings, office workers, and visitors to the area will facilitate the redevelopment of surrounding blocks for years to come.

**5** Third Street, from Bonneville Avenue to Fremont Street Experience, should be developed as a major pedestrian linkage to unite the Office Core District with Casino Center. Widened sidewalks and a greatly enhanced pedestrian experience will contribute to the revitalization of this central downtown area. The pending future vacation of the old Clark County Courthouse and other vacant public holdings present a great opportunity to redevelop several half or full blocks along the Third Street Pedestrian Connector. In the future, consideration should be given to carrying Third Street improvements south of Bonneville as commercial and new residential development warrants in the Downtown South District.

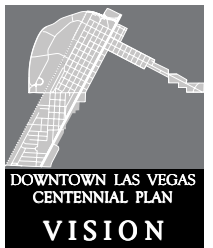
**6** Fremont Street Experience, running between Main Street and Las Vegas Boulevard, serves as a major east-west pedestrian connection, linking most major downtown casinos together and providing a pleasant, themed entertainment and strolling environment. It is a major local landmark and downtown destination center with its high-tech canopy providing entertainment by night and a shaded, breezy streetscape for daytime strolling. The development of Neonopolis, an urban destination entertainment center, coupled with the retail /family entertainment tenanting of the Fremont Street Garage frontage, will anchor the eastern end of Fremont at Las Vegas Boulevard and certainly encourage further redevelopment of the surrounding area. Strong consideration should be given to creating a pedestrian and visual linkage to the Union Pacific site as it develops to the west of Fremont Street Experience and the Office Core District. Fremont Street Experience could also be extended eastward for one or two blocks.

**7** Charleston Boulevard and Bonneville Avenue (or the proposed Bonneville / Clark paired one-way couplet) will serve as primary east-west downtown connectors, linking downtown to interstates and surrounding neighborhoods. Since Alta Avenue is soon to be widened to the west of the Union Pacific property, a gateway feature should be built as Bonneville Avenue enters downtown at Main Street. Charleston should also feature a gateway entrance near its intersection with Main Street and include special landscaping, lighting, and signage to make it distinctive.

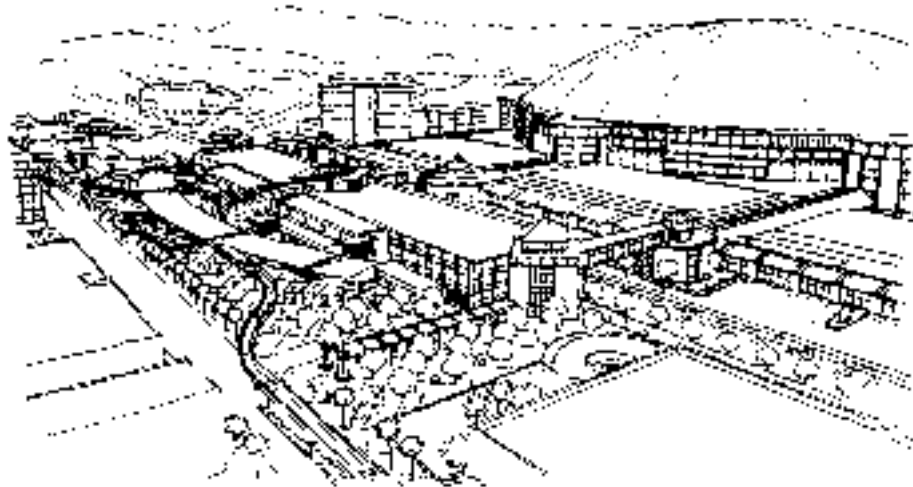


8 The most pivotal linkage in downtown will occur on Main Street. It is postulated to anchor the alignment of a fixed guideway transit system, linking downtown to Cashman Field on the north and The Strip to the south. Stations scheduled along Main Street will benefit the Fremont Street Experience, Office Core District, development of the Union Pacific site, Downtown South District, and Northern Strip area. Main Street will continue to serve as an alternative route for local north-south downtown traffic. The possible future connection of Main Street with Industrial Boulevard to the south of downtown could provide a continuous service axis to The Strip's western backdoor.

9 The net effect of all linkage systems and improvements will result in drawing people from one place to another, improving the image and character of the area, and consequently encouraging a greater use of Downtown Las Vegas in its entirety.



GRAPHIC 7: A HYPOTHETICAL FIXED GUIDEWAY STATION LINKING INTO A STADIUM OR ARENA

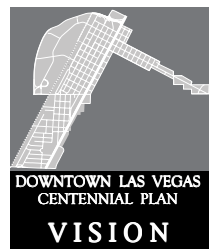


### V3E. PROPOSED MIXED USE OPPORTUNITIES

**1** Creating a broad diversity of land uses in Downtown Las Vegas is key to its future success as an activity center and vital urban regional hub. It should have a hearty balance of cultural, residential, commercial, office, civic, light industrial, and special uses.

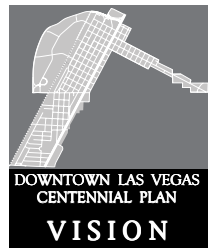
**2** Residential Neighborhoods: The success of downtowns across the country often depends upon integrating a successful residential community in, among, and adjacent to the downtown area. Las Vegas has a great opportunity to integrate a stronger residential community into the future of Downtown Las Vegas by building upon the existing infrastructure already in place. The City has identified the potential of building some 5000 residential units over the next 10 years within downtown Las Vegas. The creation and strengthening of neighborhoods is essential, except along Las Vegas Boulevard. It needs more than just the development of housing types that may focus inwardly without integrating to the physical and social structure of the surrounding community. Creating a neighborhood would include providing improved police protection, parks, schools, community centers, shopping, and other similar uses. The successful integration of residential development into Downtown Las Vegas will require focusing on efforts in specific areas, establishing a critical mass, and then transitioning to other areas one zone at a time. Individual successes will build upon the previous effort, and the whole of the community will be built over time, utilizing the momentum gained from the earlier successes.

**3** Parks and Open Space: Many downtowns across the nation are recognized for their formal public open space, such as William Penn's four urban greens breaking the grid of Philadelphia, Washington Square in Manhattan's Greenwich Village, and Boston's Common. Over the next few years, the City should pursue the creation of a series of small urban park spaces linked by trails and enhanced pedestrian streetscapes to expand the livability of Downtown. Several near future opportunities are looming. The pending federal disposition of the 1933 United States "Old" Post Office Building at Stewart and Third will present the City with the opportunity to reprogram it for civic and cultural uses, and redevelop the site as green space. The east-west Bonanza Trail should be provided with a trailhead to link with the Old Post Office and the north-south Third Street Corridor into Downtown. Undeveloped blocks on the south side of Fremont Street at Maryland Parkway afford the chance to devise a full block urban park and introduce new residential uses to center the East Fremont District. The



redevelopment potential of the Downtown South District suggests the strong desire to create an entire urban block as a formal park, and surround it with new residential and mixed-use construction. Third Street should be rebuilt (from Fremont to Charleston) to have spacious sidewalks, shade trees and decorative plantings, thus serving as the primary north-south pedestrian route linking the Old Post Office to Fremont Street Experience, to the Lewis Street Corridor, to the Downtown South District park. (Fourth Street and Casino Center will serve the primary function of carrying northbound and southbound vehicular traffic respectively.) The development of a jogging and exercise trail as a green buffer between Parkway Center and I-15 should also be considered.

**4** Hotels & Casinos: Casino use is of particular importance to the history and character of Las Vegas. Downtown must ensure its role in the casino enterprise that is so much a part of local culture and national folklore. The downtown casino areas not only preserve the history and culture that has already been established, but ensure vitality and synergy among existing and future casinos. The casino areas delineated in this Plan illustrate areas that build upon and preserve those historical patterns while providing the opportunity for future expansion.

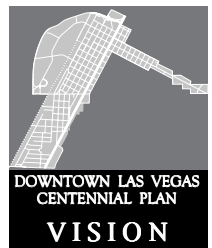


**5** Government Offices: Downtown Las Vegas is host to the City Hall complex and will soon house the new Regional Justice Center, Federal Courthouse, and Detention Center expansion. Other City and County offices are currently dispersed across downtown. These significant new projects, in conjunction with the expansion of the City Hall complex, require that the City establish appropriate strategies for accommodating a significant civic core downtown. The complex of structures should display a high quality architectural prominence and surround a significant civic plaza that would serve as a focal point for public activity and events.

**6** Commercial Office Core: The primary Office Core for Downtown Las Vegas extends from approximately Garces Avenue on the south to Carson Street on the north and is bounded by the Union Pacific tracks on the west and Las Vegas Boulevard on the east. This area illustrates an overall district consisting primarily of office uses with the majority of large-scale, class A office space to be accommodated in its northern half. The northern portion of the overall district, or Office Core, can best be described as the central business district. It will be home to governmental and corporate office uses and support retail, restaurant, and other service uses necessary to complete an active Office Core. The southern half of the office district will be smaller scale office uses with predominantly single users in renovated structures or small office buildings. Other uses may be found in conjunction with this type of office use including residential, commercial, and support services.

**7** Light Industrial / Service Commercial: This area will be comprised of a mix of industrial and commercial services such as automotive service, paint shops, light manufacturing, laundry / cleaners, copy center, and similar types of uses. Its relationship to the railroad tracks and freeway system will continue to support light industry as well as facilitate commercial development in transitional areas nearer Las Vegas Boulevard and other arterial streets. Design standards should be established for the industrial area, particularly as it abuts the residential and casino areas, and along the fixed guideway system alignment. Its visual impact and the potential for disruptive features such as noise from trucks and trains, chemical usage, and other factors will create potential harmful and detrimental effects for users of adjacent properties.

**8** Commercial Corridors: The primary Downtown Las Vegas commercial district encompasses the Charleston and Las Vegas Boulevard corridors. It will be comprised of traditional Las Vegas commercial uses such as hotels and motels, wedding chapels, photographic studios, service commercial including convenience retail, specialty retail and other similar uses.



**9** Parkway Center: The former Union Pacific railyards site to the west of the Office Core District have been rezoned in 1998 to Planned Development (PD), from its former Industrial (M) zone. The City anticipates a variety of land uses developing on the site, including non-gaming hotels; convention and arena facilities; and retail, residential, cultural, and mixed uses. Clark County will also continue to expand its government services campus. Pedestrian linkages to Fremont Street Experience and the Office Core will be very important to the City: Parkway Center must function as a natural extension of the existing downtown to enhance the businesses that already exist there. A tremendous opportunity is now emerging to develop an intermodal transportation center that would link together the fixed guideway, the Amtrak, Transrapid high speed train and, the CAT bus system with other existing forms of transportation. Such a station could also serve as a bridge between Fremont Street Experience or the Office Core and Parkway Center. It is recommended that this site be planned as a whole and not sold or developed on a piecemeal basis.



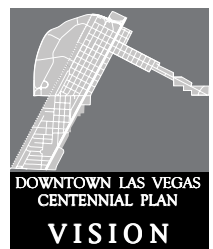
## V4. DEFINITION OF DOWNTOWN DISTRICTS

**1** For the purposes of this Plan, Downtown Las Vegas has been divided into seven distinct and recognizable districts (Map 6). Each district has a particular variety of land uses, density, and urban character demonstrated by the existing building fabric it contains and the redevelopment opportunities it presents. All seven districts are small enough to be walkable enclaves unto themselves, yet the continuity of general design standards and streetscape design will weave them together into a continuous downtown urban experience. Downtown Las Vegas excludes the Las Vegas High School Historic District.

**2** District One: Office Core - The Office Core District, which is the primary concentration of professional and governmental offices and supporting pedestrian-oriented uses, is bound by the eastern edge of the Union Pacific Railroad right-of-way on the west, the centerline of Bridger Avenue between the tracks and Casino Center Drive, but then the centerline of Carson Avenue between Casino Center Drive and the properties on the eastern side of Sixth Street on the north, the centerline of Sixth Street on the east, and the centerline of Garces Avenue on the south (Map 6).

**3** District Two: Las Vegas Boulevard - The Las Vegas Boulevard District overlays all the other districts and is the primary tourist commercial and entertainment-directed spine that links The Strip to Downtown Las Vegas and its varied districts. The district is generally defined to include the Las Vegas Boulevard right-of-way and all the properties adjacent thereto on both the west and east sides of the right-of-way (Map 6).

**4** District Three: Downtown South - The Downtown South District is a distinct mixed-use area containing a wide variety of land uses and density of existing utilitarian buildings and bungalows ripe for rehabilitation. It will continue to redevelop and contain an eclectic and urbane mix of businesses and residences including artist studios; cafes and coffee houses; art galleries; small professional offices; artisan and craftsperson storefronts and workshops; lofts and live / work situations; and diverse mixed-use developments within existing buildings and new developments. The district is bound by the eastern edge of the Union-Pacific Railroad right-of-way on the west, the centerline of Garces Avenue on the north, the properties on the eastern side of Sixth Street to Charleston Boulevard but then the eastern side of Las Vegas Boulevard south of Charleston Boulevard on the east, and the centerline of Wyoming Avenue on the south. The Downtown South District also includes the sub-district now commonly referred to as The Arts District surrounding the Arts Factory building, and the area some are now

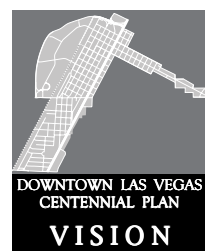


calling the Gateway Park Neighborhood to the north and west of the Gateway Park entry feature, at the split of Fourth Street and Las Vegas Boulevard. (Map 6)

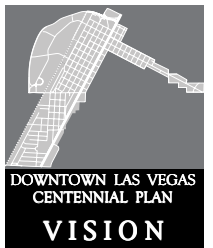
**5** District Four: Parkway Center - Parkway Center encompasses the entire former Union Pacific railyards, which in part is home to the Clark County Government Center. The remainder of the site is vacant and prepared for redevelopment opportunities, such as a performing arts center, a professional sports stadium, offices, high density residential, and commercial uses. Parkway Center should become a dense, walkable, mixed-use environment linked to the existing downtown street grid. The district is bound by the eastern edge of the Interstate 15 right-of-way on the west, the southeastern edge of the “Spaghetti Bowl” and U.S. 95 / Interstate 515 on the north, the western edge of the Union-Pacific right-of-way on the east, and the centerline of Charleston Boulevard on the south (Map 6).

**6** District Five: Casino Center - The Casino Center District is home to the historic and classic Las Vegas casinos of Glitter Gulch, the fabulous Fremont Street Experience light and sound extravaganza, and the new and exciting Neonopolis. The District is also home to Las Vegas City Hall, the Downtown Transportation Center, the Old Post Office, and other related land uses. Casino Center is bound by the eastern edge of the Union-Pacific right-of-way on the west, U.S. 95 / Interstate 515 on the north, the properties on the eastern side of Seventh Street between Interstate 515 and Ogden Avenue, but then the centerline of Las Vegas Boulevard between Ogden Avenue and Carson Avenue on the east, the centerline of Bridger Avenue between the tracks and Casino Center Drive but then the centerline of Carson Avenue between Casino Center Drive and Las Vegas Boulevard on the south (Map 6). (The remaining areas north of Interstate 515 that are within the CCDC Area of Influence but not now included within this district could be added to this district in the future or combined with additional parcels to the north to form a future Downtown North District.)

**7** District Six: Northern Strip - The Northern Strip District centers around the Stratosphere Tower Hotel and Casino and includes the old Meadows Village Neighborhood to its west. This transitional district is the ideal location for future major hotel-casino projects adjacent to the Stratosphere and also along Sahara to seamlessly extend The Strip northward into Downtown Las Vegas. Future evaluations will have to determine the highest and best land uses for this area. The district boundaries are the eastern edge of the Union-Pacific right-of-way on the west, the centerline of Wyoming Avenue on the north, the properties on the eastern side of Las Vegas Boulevard on the east and the centerline of Sahara Avenue on the south (Map 6).



**8** District Seven: East Fremont - The East Fremont District is the logical extension of the Fremont Street Experience eastward and includes the El Cortez Hotel and Casino, other small casinos, and supporting commercial, motel, and residential uses. This transitional district is bound by the centerline of Las Vegas Boulevard on the west, the centerline of Ogden Avenue on the north, the centerline crossing of Eastern Avenue and Charleston Boulevard on the east and the centerline of Carson Avenue on the south. The East Fremont District will present future opportunities for new commercial ventures and the integration of medium density mixed-income residential neighborhoods. The potential for open space to center these new neighborhoods should be investigated in the area of Fremont Street and Maryland Parkway (Map 6).

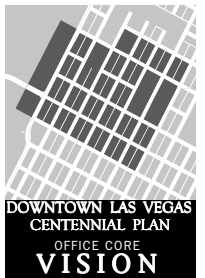


## OFFICE CORE DISTRICT: VISION

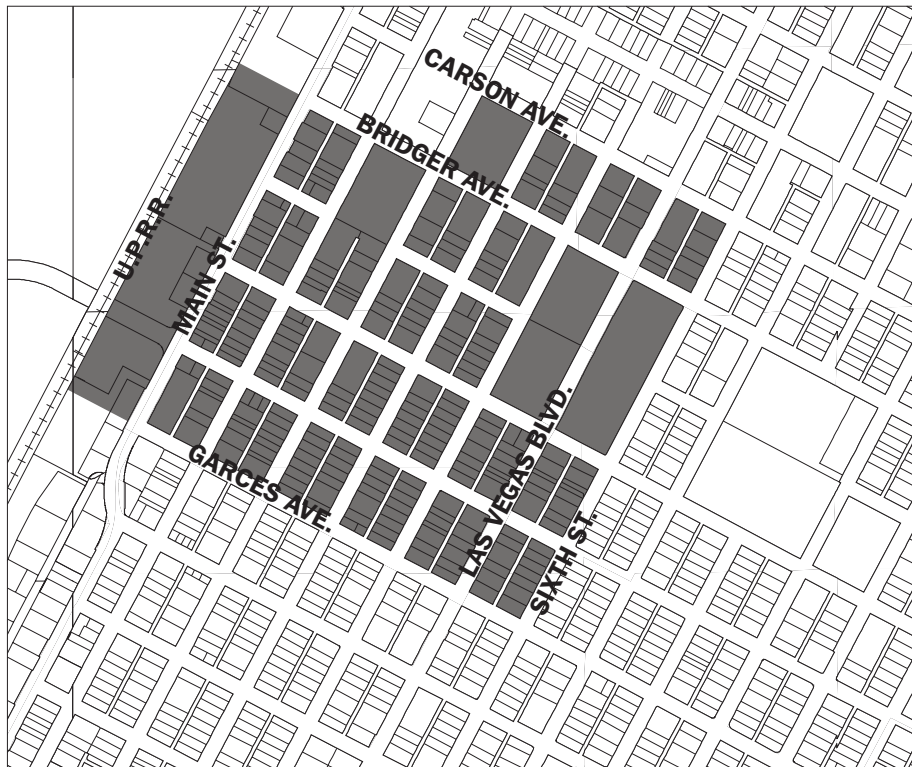
### V5. OFFICE CORE: BOUNDARIES

**1** The Office Core District shall be defined as the area bound by the eastern edge of the Union-Pacific Railroad right-of-way on the west, the centerline of Bridger Avenue between the tracks, and Casino Center Drive, but then the centerline of Carson Avenue between Casino Center Drive and Sixth Street on the north, on the east the centerline of Sixth Street, and the centerline of Garces Avenue on the south (Map 7).

**2** Office Core District Plan and Policies follows hereafter.



MAP 7: OFFICE CORE: DISTRICT BOUNDARY

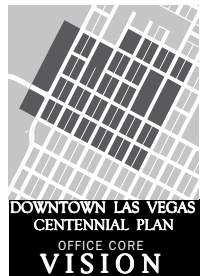


## V5A. OFFICE CORE: EXISTING CONDITIONS

**1** The Office Core District is composed of a mix of uses; including corporate office towers; Clark County government offices and court complex: a Federal Courts complex; an historic school now being used for commercial and governmental office space; a detention center; small hotels, churches, and assorted residential structures.

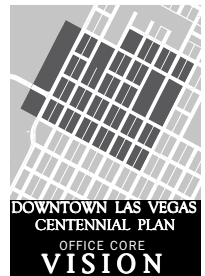
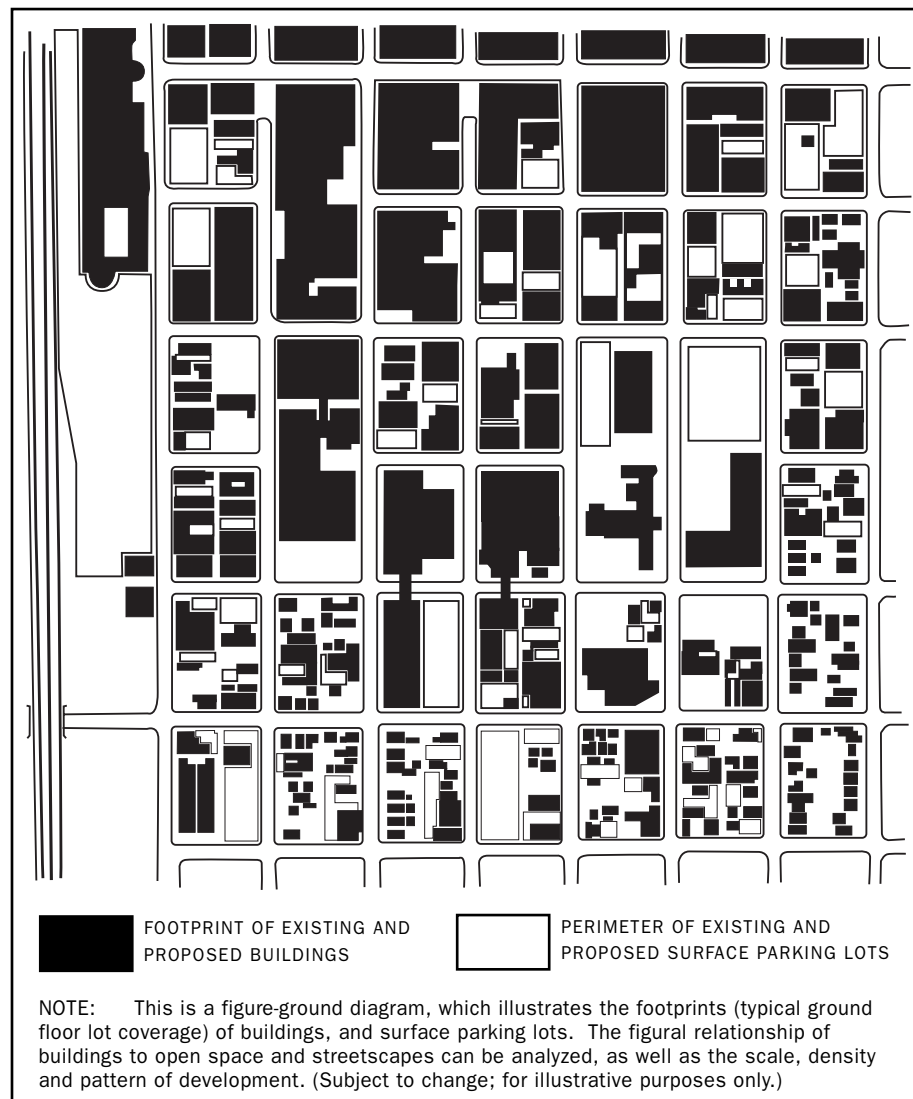
## V5B. OFFICE CORE: MARKET DEMAND

**1** Until recently, Downtown Las Vegas and the Office Core District has not seen any significant new office construction (over 50,000 SF) since 1986. According to real estate development advisors, Keyser Marston Associates, Inc., there is ample market support for downtown office development in the future (up to 330,000 SF by 2001, an additional 350,000 SF to 2006). With the correct development strategies employed, Downtown Las Vegas could expect to capture all that demand and more.



1 Existing development patterns in the Office Core illustrate the historic land ownership of small, individual parcels mixed throughout the area. A few blocks have been redeveloped beyond those existing patterns, and current construction projects and development proposals will alter another seven or so blocks. The development patterns in the southern portions of the Office Core still reflect the small, historic ownership patterns while the northern portions of the Office Core illustrate the more recent development patterns showing larger buildings on larger parcels (refer to Map 8 for an illustration of existing development patterns).

MAP 8: OFFICE CORE: EXISTING URBAN FORM PLAN WITHIN AND SURROUNDING THE OFFICE CORE DISTRICT



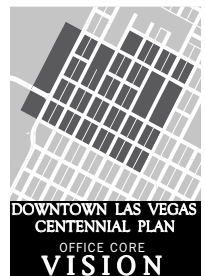
**2** The small parcel size and individual ownership patterns in the Office Core create a difficult scenario in which large-scale redevelopment efforts can be successful. It is difficult to find parcels of adequate size or to assemble parcels of adequate size for development of a quarter to a half block in size or larger. Many existing land owners are holding onto properties that they mistakenly believe have a potential speculative value for casino development, which is significantly higher than for office development. This fact, combined with numerous property ownerships, creates a very difficult situation for any future office development to occur in the Office Core. Clear and revised delineation of the Gaming Enterprise Overlay would redirect land values within and surrounding the Office Core District.

**3** The City should work with the Clark County Assessor to determine the appropriate and fair assessments of parcels and improvements within the downtown area, increasing assessments to at least 90 percent of current market value, or comparable. This will encourage divestiture of under utilized parcels and assist with assembly of small lots into useful redevelopment sites.

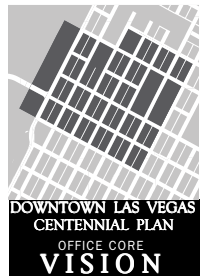
**4** It is the intention and goal of the City to encourage the development of new complementary urban development within the Office Core District. Desirable unifying elements can include building design, size, shape, mass, height, color, and materials. This will, over the next generation, overcome the seemingly random pattern of development that has occurred previously in the Office Core District.

**5** There are currently few amenities for the pedestrian in the Office Core. The recent completion of the Fremont Street Experience and the Fourth Street Corridor landscape improvements have added great value to what was a generally harsh pedestrian experience. The Office Core lacks parks, open space for public gatherings or lunch-time activities, trees, benches, and attractive surface treatments for walkways.

**6** The Office Core does not currently boast a great diversity of uses. Most current uses consist of offices oriented toward the courts and judicial activities. Small support uses such as copy centers, banks, and other similar uses are found sporadically throughout the denser development areas. It is the intention and goal of the City to encourage new class A professional office development, a rich variety of retail and commercial storefronts, restaurants and cafes, mixed-use and residential developments within the Office Core District.



**1** The existing street system in the Office Core has adequate capacity to accommodate the anticipated future demands of potential new development. There are some capacity constraints in specific segments and at specific points and some lack of network continuity to address. For many years, there has been no consistent zoning requirement for parking in the Office Core. Existing parking on streets, surface lots, and in garages is at practical capacity with little or no room to absorb future large scale development parking needs. Little attention has been given toward transportation management strategies such as demand reduction through ride-sharing and other programs. Bus turnouts along all bus routes, but especially Las Vegas Boulevard, are strongly encouraged where right-of-way acquisition permits.





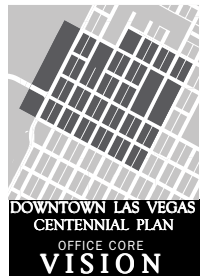
## V5E. OFFICE CORE: STRATEGIES

### V5E1. OFFICE CORE: MARKET STRATEGIES

- a. Assemble large development parcels suitable for large-scale redevelopment projects.
- b. Establish amenities for Office Core tenants, workers, and visitors. Provide ample, affordable parking, lighting, landscaping, parks / open space, and encourage support uses such as restaurants and retail.
- c. Casino development shall be limited to two finite casino areas outside the Office Core: Fremont Street Experience / Casino Center, and The North Strip area along Las Vegas Boulevard between Sahara and Charleston, pursuant to the revised Casino Enterprise Overlay District. Target future casino development for these two distinct districts.

### V5E2. OFFICE CORE: LAND USE AND URBAN DESIGN STRATEGIES

- a. Parkway Center is a Planned Development (PD) zoned District requiring a master development plan and design standards. The master development plan and standards will establish appropriate land uses, minimum and maximum Floor Area Ratios (FAR's), parking strategies, open space, streetscapes, and relationships to the Downtown Las Vegas area east of the site.
- b. Establish a system of arcades within and beyond the Office Core, recapturing a sense of the City's historic past, reinvigorating the streetscape, providing comfort for pedestrians, and creating a unique and identifiable image.
- c. Establish prominent civic squares or parks within or near the Office Core. A park or square could be combined with the possible construction of a new City Hall.
- d. Establish one or more residential neighborhoods at the edge of the Office Core, which will induce related shopping, recreation, restaurant, and service uses in the area, including a 24-hour lifestyle.



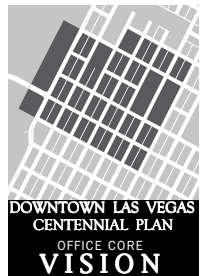
### V5E3. OFFICE CORE: TRANSPORTATION STRATEGIES

- a. Complete the second half of the paired one-way couplets on Casino Center southbound beginning south of Bridger Avenue, perhaps at Clark Avenue.
- b. Connect Main Street with Industrial Road to create a back road service route for the casinos.
- c. Explore the potential to connect St. Louis Avenue with Oakey Boulevard.
- d. Implement signage improvements, both freeway and internal.
- e. Upgrade existing signal system and add new signals at specified intersections.
- f. Limit any further street closures in the Office Core and surrounding area.
- g. Establish a CCDDC and / or City parking strategy and unified Downtown Parking Plan to develop a system for the unified provision and management of centralized off-street and on-street parking.
- h. Expand alternative modes of transit for long-term benefits. This includes:
  - 1. Designating Main Street for a preferred fixed guideway alignment corridor
  - 2. Developing an intermodal transportation center
  - 3. Promoting incentives for transportation reduction including ride-sharing, pedestrian systems, preferred ride-share parking, etc.
- i. Targeting the visitor market via future transit investments

**1** The Urban Design Principles provide a framework and graphics for the Office Core in order to achieve the ultimate goal of repositioning Downtown Las Vegas as an attractive, safe, and economically vital regional center for business, commerce, government, entertainment, and the arts.

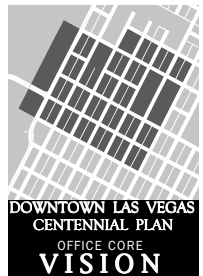
#### V6A. OFFICE CORE: URBAN FORM PRINCIPLES

- a. Respond to the overall urban structure and infrastructure to the extent possible. Re-alignment of streets, street closures, or other significant infrastructure changes shall be discouraged.
- b. Strive to seek large-scale redevelopment opportunities (quarter to half block in size or larger) in the Office Core of Sixth Street west through consolidation of the historically small development parcels and ownership patterns.
- c. Minimize large “gaps” in the urban fabric as a result of surface parking lots or under utilized parcels. Maximizing the available land through consolidated redevelopment efforts is preferred over the random, scattered, small-scale patterns that have historically taken precedent.

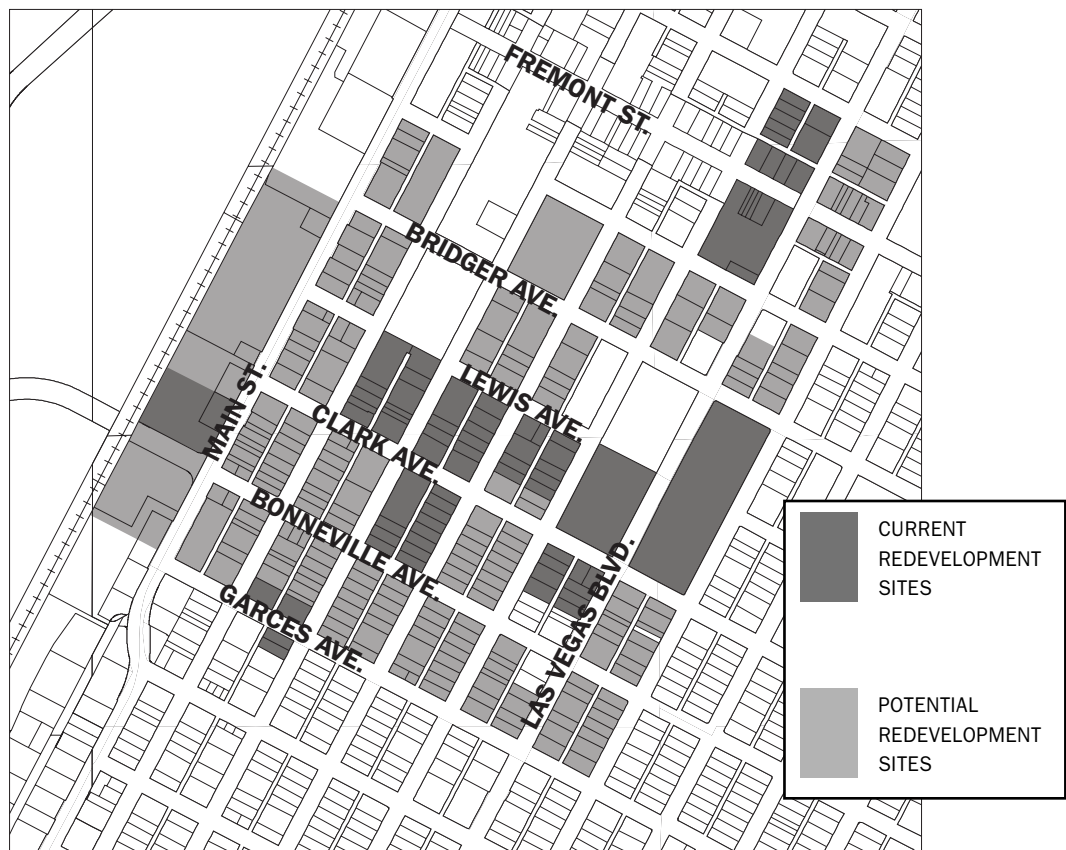


**1** The Office Core Potential Redevelopment Areas Plan targets those areas in the Office Core which should become the focus for future redevelopment efforts (Map 9). The blocks highlighted generally represent redevelopment potential because they meet any one or all of the following conditions:

- a. Comprised of historically small parcelization and land ownership patterns currently represented by small structures or vacant lots.
- b. Hosting structures which have reached a point where they are no longer competitive in the Class A office market.
- c. Properties that demonstrate a need for redevelopment uses that support a synergistic relationship to the Office Core District (i.e. business restaurants, hotels, entertainment venues, etc.)
- d. Hosting structures that lack historical or culturally significant value.



MAP 9: OFFICE CORE: POTENTIAL REDEVELOPMENT AREAS PLAN WITHIN AND SURROUNDING THE OFFICE CORE DISTRICT



## V6C. OFFICE CORE: ZONING

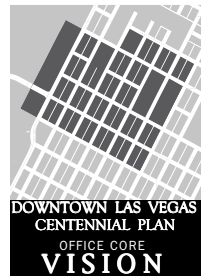
**1** Pursuant to Title 19A, City of Las Vegas Zoning code, as amended, the Office Core District is primarily zoned C-2, General Commercial, which allows a wide variety of commercial uses. Some parcels are now zoned C-V, Civic, which allows particular civic uses such as courthouses, public offices and the detention center. A small number of parcels may still be zoned C-1, Limited Commercial, and R-4, High Density Residential, which reflects the existing land uses of those parcels.

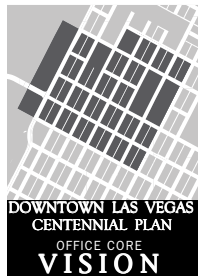
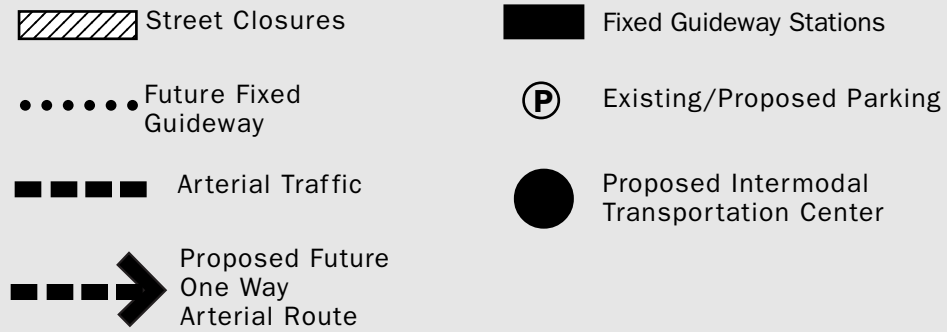
**2** As redevelopment occurs, it will be appropriate to rezone sites for commercial use to C-2 and sites for public facilities to C-V. It is the intention of this Plan to allow the entire Office Core District to redevelop as commercial office and mixed-use development, with the possible incorporation of very high density residential uses.

## V6D. OFFICE CORE: VEHICULAR TRANSPORTATION / ACCESS

**1** The Office Core Transportation / Access Plan (Map 10) illustrates the primary vehicular network considerations for the Office Core. Of particular note are the following strategies:

- a. Keep street closures to a minimum where possible.
- b. Incorporate the proposed fixed guideway transit system along Main Street with stations along Main at Fremont Street Experience and Clark Street.
- c. Complete the north-south one-way paired couplet system in the Office Core and throughout Downtown with Fourth St. being one-way northbound and Casino Center Boulevard being one-way southbound beginning at Bridger Avenue.
- d. With Alta Avenue widening planned to the west of Downtown, consider developing an east-west one-way paired couplet system in the Office Core and beyond, with Bonneville Avenue becoming one-way eastbound and Clark Avenue becoming one-way westbound. The block bound by Main, Clark, First and Bonneville would have to be redeveloped to allow Clark to transition toward the new Bonneville underpass.
- e. Establish shared parking structures to provide the opportunity of common shared facilities that generate pedestrian movement between parking and land uses.
- f. Establish a City Parking Authority to provide a unified supply and management of on-street and off-street parking in the Office Core District and throughout Downtown. The Parking Authority's responsibility would include planning, design, construction, financing, land acquisition, operation, monitoring, and enforcement of all public parking in Downtown Las Vegas.

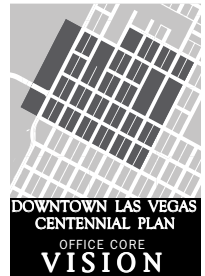




MAP 10: OFFICE CORE: TRANSPORTATION / ACCESS WITHIN AND SURROUNDING THE OFFICE CORE DISTRICT

**1** The Office Core Primary Pedestrian System Plan (Map 11) illustrates a hierarchy of pedestrian improvements to be established in the Downtown Las Vegas area. It will be particularly important to:

- a. Establish Lewis Street as a primary east-west pedestrian corridor in the Office Core linking the Union Pacific site on the west to the old high school on the east. The Lewis Street Corridor should have distinctive paving treatment, street widths, landscaping, lighting, and signage along its length in the Office Core. It should also incorporate a significant civic plaza at Third Street in conjunction with proposed developments.
- b. Extend the landscaped median treatment on Las Vegas Boulevard from the Strip to its intersection with Interstate 515.
- c. Establish Third Street as a primary north-south pedestrian linkage in the Office Core. It will perhaps have a landscaped median or widened landscaped sidewalks and distinctive lighting, signage, and paving. It will serve primarily as a pedestrian conduit for movement from the Lewis Street Corridor north to the Fremont Street Experience and southward into new residential and mixed-use neighborhoods.
- d. Maintain Fourth Street as an enhanced pedestrian and vehicular route through the Office Core. It will continue as a one-way northbound vehicular connector with its own landscaping, lighting, and special paving treatments.
- e. Establish an overall landscape theme for the Office Core consisting of north-south streets lined with palm trees and supporting landscape elements, and east-west streets lined with shade trees and supporting landscape elements.
- f. Establish pedestrian connections over / under the Union Pacific railroad tracks linking the Office Core District with the Downtown West District and future development activities.



## V6F. OFFICE CORE: PARCEL BUILD-TO LINES AND STREETWALLS

**1** The Office Core Streetwall and Parcel Build-To Lines (Map 12) strengthen the primary pedestrian network by requiring that buildings contribute to the sense of place through consistent massing, building heights, and their frontage along pedestrian corridors at the sidewalk and street edges.

**2** The parcel build-to-lines are designated along the primary pedestrian corridors along Lewis Avenue, Bridger Avenue, Clark Avenue, Third Street and Las Vegas Boulevard. These are designated such that building facades will help define the street and that uses at the ground floor will contribute to animating the street through pedestrian activity. Minimum parcel sizes and Floor Area Ratios determine the bulk of a potential structure ensuring that small, singular uses do not continue to dominate the Office Core. Seventy percent (70%) of the building mass facing the streets and entries should abut the build-to line. Minor variations in facade articulation for columns, pil-



lars, etc., will be allowed and applied toward abutting requirements. The build-to line pertains only to the first three stories or 45 feet, whichever is greater, of the building.

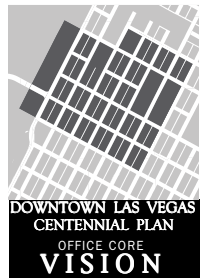
**3** Structures shall have a consistent relationship to one another. A consistent streetwall encourages pedestrian movement. The consistency in urban form will help delineate and define the overall urban character of the Office Core and strengthen the visual imagery. The areas not delineated for setbacks, building height, or Floor Area Ratio (FAR) requirements may have design freedom in accordance with other design standards for the Office Core outlined later in this document.

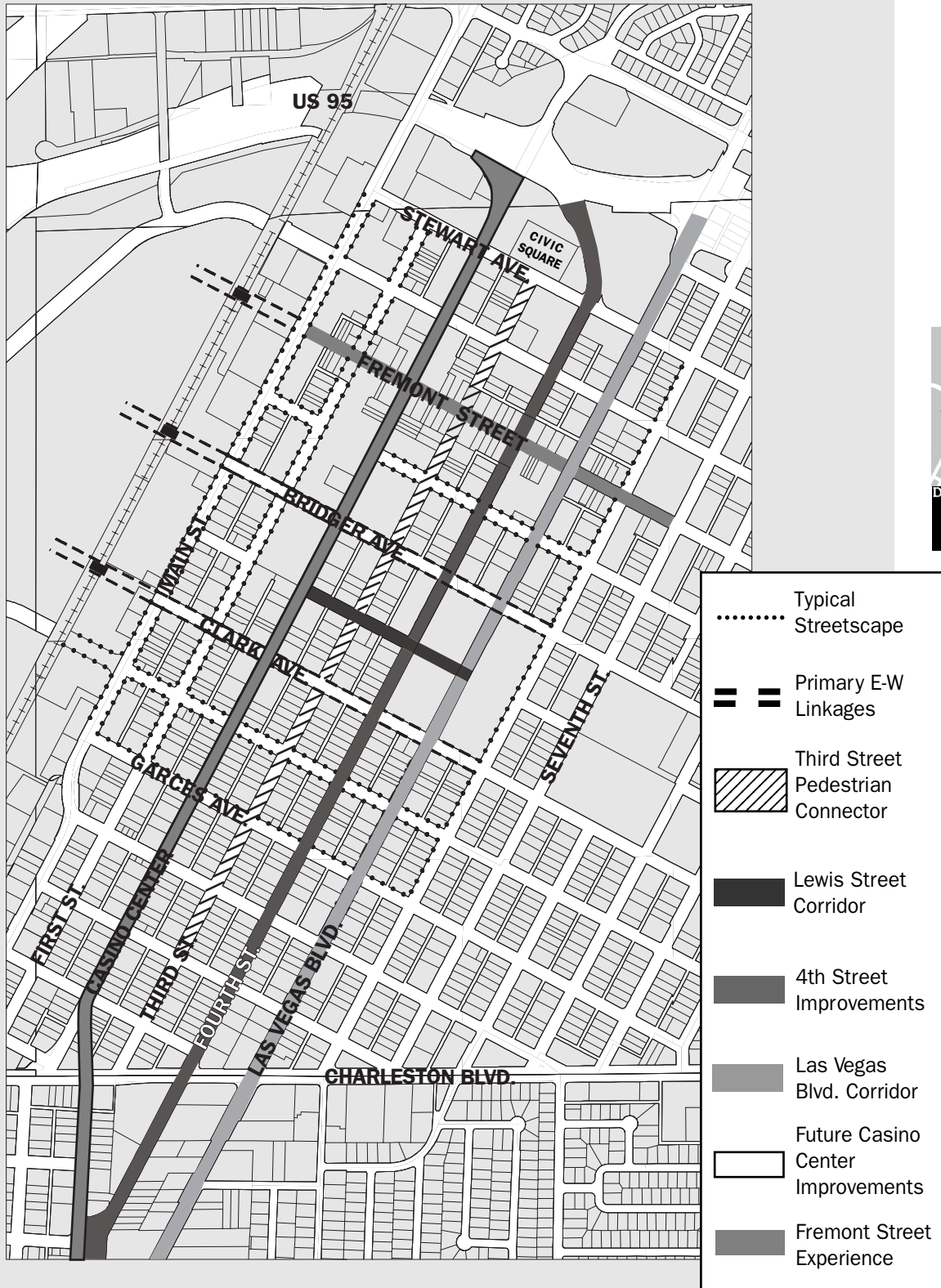
#### V6H. OFFICE CORE: GROUND LEVEL USES

**1** The Office Core Ground Floor Special Use Plan (Map 13) illustrates those areas that must have standards for use at the ground level to contribute to activity at the street level.

**2** Fremont Street, Bridger Avenue, and Clark Avenue have been identified as the primary east-west commercial streets, because of the potential for connectivity to the Downtown West District and the proposed location of Fixed Guideway Stations along Main Street at Fremont and Clark. Main Street, Third Street and Las Vegas Boulevard are identified as the primary north-south commercial streets.

**3** In the areas designated Retail / Commercial, seventy percent (70%) of the building at the street level should have uses oriented toward retail, entertainment, and / or commercial activities. This will provide for uses that will generate pedestrian activity as well as night-time activity generated by restaurants and entertainment venues. To provide interaction between the building and the street, penetrations via doorways, porticoes, or other openings into the building from the street must occur a minimum of every 75 feet of facade fronting the street or plaza areas in this zone.





MAP 11: OFFICE CORE: PRIMARY PEDESTRIAN SYSTEM WITHIN AND SURROUNDING THE OFFICE CORE DISTRICT

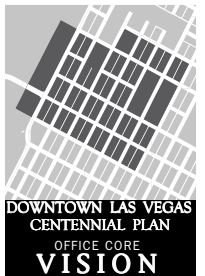




Recommended Build-To Line  
(at Property Line)

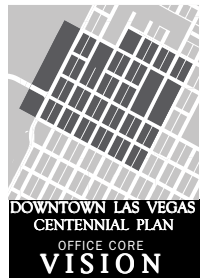


Recommended  
Special Pedestrian Features



MAP 12: OFFICE CORE: STREETWALL AND PARCEL BUILD-TO LINES WITHIN AND SURROUNDING THE OFFICE CORE DISTRICT

-  Retail / Commercial Frontage Desired at Sidewalk
-  Fremont Street Experience Corridor
-  Neonopolis, Urban Destination Entertainment Center
-  Fifth Street School Potential Re-Use



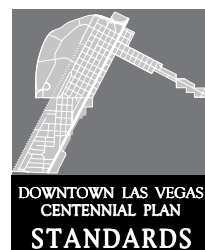
MAP 13: OFFICE CORE: GROUND FLOOR SPECIAL USE REQUIREMENTS WITHIN AND SURROUNDING THE OFFICE CORE DISTRICT

## DS. DOWNTOWN LAS VEGAS: DESIGN STANDARDS

### DS1. URBAN DESIGN GOALS AND OBJECTIVES:

- 1** These are the design goals and objectives for land use in Downtown Las Vegas:
- Create retail and other “pedestrian-friendly” uses on the ground floor of buildings, particularly in identified districts and corridors.
  - Create a better definition of the districts in and around downtown for more consistent, managed design character and image related to the uses therein, and to achieve a compatibility of uses between and among districts.
  - Encourage new housing in downtown where appropriate.
  - Encourage diversity of land uses within a guiding framework to achieve long-term vitality and economic growth.

- 2** These are the design goals and objectives for urban form in Downtown Las Vegas:
- Bring building facades to the property line / sidewalk edge along primary pedestrian streets.
  - Require landscaped setbacks along primary vehicular streets.
  - Require innovative treatments related to parking structures, incorporating retail or commercial uses into the ground floor and restricting facade treatments to appropriate architectural finishes.
  - Develop districts with consistent and cohesive architectural massing, establishing development standards for Floor Area Ratio (FAR), building heights, and streetwall formation.
  - Capitalize on recent development momentum created by the opening of the Fremont Street Experience, the proposed Regional Justice Center, Federal Courts Building, Neonopolis, and the potential expanded City Hall complex.



**3** These are the design goals and objectives to create a pedestrian environment in Downtown Las Vegas:

- a. Create streetscapes that provide safety, comfort, and interest for pedestrians.
- b. Establish a network of urban open spaces consisting of corridors, nodes, and linkages.
- c. Encourage the provision of arcades, particularly in the Office Core.

**4** These are the design goals and objectives to enhance image and character in Downtown Las Vegas:

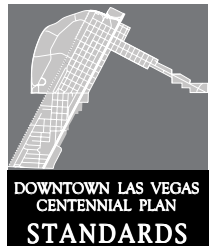
- a. Enhance the overall image of downtown through improvements at gateways, primary corridors, civic plazas, and open spaces.
- b. Develop an alley beautification and management plan.
- c. Establish a sense of place based on regional culture, climate, and environmental elements unique to Downtown Las Vegas.

**5** The following Design Standards apply throughout all of Downtown Las Vegas and are applicable to all seven districts.

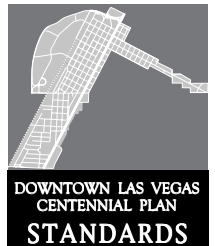
**6** The purpose and intent of establishing Downtown Las Vegas design standards is to restore vitality and economic opportunities, give overall direction to future public and private improvements throughout the area. When applied, they will create a downtown that will have a unique, identifiable, high quality, vibrant image and character. The standards will enrich the pedestrian experience downtown through improved streetscapes, landscaping, parks, plazas, and open space. These design standards, in conjunction with the land planning strategies, will redefine Downtown Las Vegas as an attractive, safe, economically vital regional center for business, commerce, government, entertainment, and the arts.

**7** Where different standards or elements are included within the District Plans contained herein, such as the Office Core District Plan, they shall supercede the Downtown Las Vegas and other existing standards.

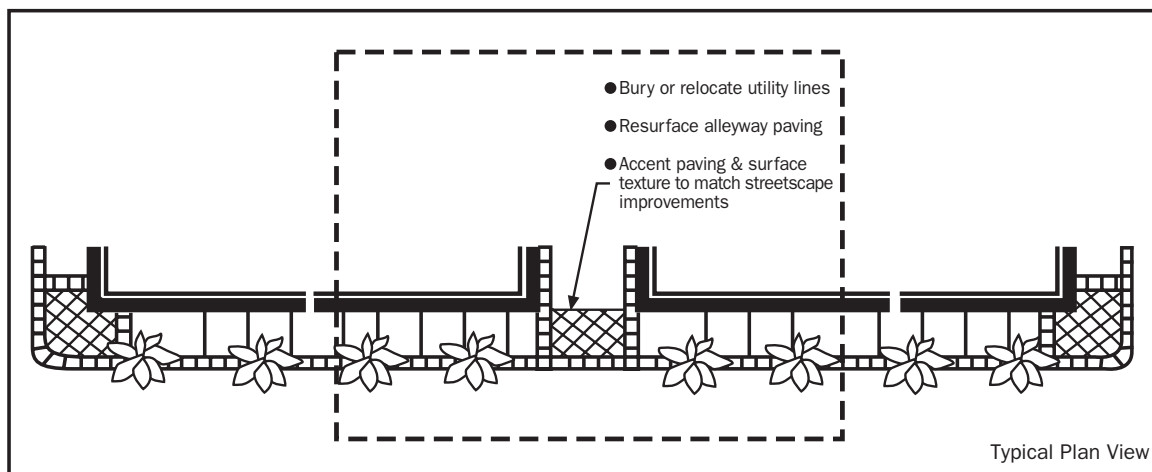
**8** Waivers to the Design Standards contained within the Downtown Las Vegas 2005 Plan may only be granted by the City Council with clear and convincing evidence for demonstrated economic hardship or to further the City's redevelopment efforts.



- 1** These are the site planning standards for all of Downtown Las Vegas:
- a. All development proposals for new buildings and structures shall conform to the minimum parcel size, if any, as specified for the area.
  - b. All development proposals for new buildings and structures shall conform to the minimum and maximum building heights, bulk, and Floor Area Ratio (FAR) requirements, if any, as specified for the area.
  - c. Plan all structures to conceal service areas and storage areas from public street view. Incorporate these uses into the main building whenever possible.
  - d. All auto-related facilities (working bays, storage, etc.) shall orient away and be screened from public view. When parking lots face major public streets, ornamental screen and landscaping shall be incorporated, as per Graphic 9.
  - e. Buildings shall orient their public entrances toward major street frontages as much as possible, particularly on major pedestrian streets.
  - f. Utilities and power service lines shall be located underground for all new development. To the extent possible, relocation of utilities from alleys (above ground) to streets (below ground) shall be undertaken with all new development activities, adjacent to their developments.
  - g. When alleyways are abandoned and building envelopes expanded to create larger development parcels and to eliminate negative character of existing alleys, the developer shall relocate utilities and design sidewalks to approved design. If the alley is not abandoned, it shall conform to the standard Alleyway Treatment (Graphic 8).

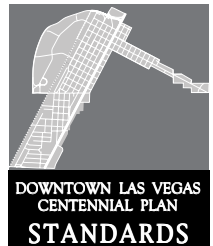


GRAPHIC 8: STANDARD REQUIRED ALLEYWAY TREATMENT, IF ALLEY NOT VACATED.

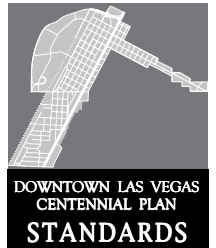
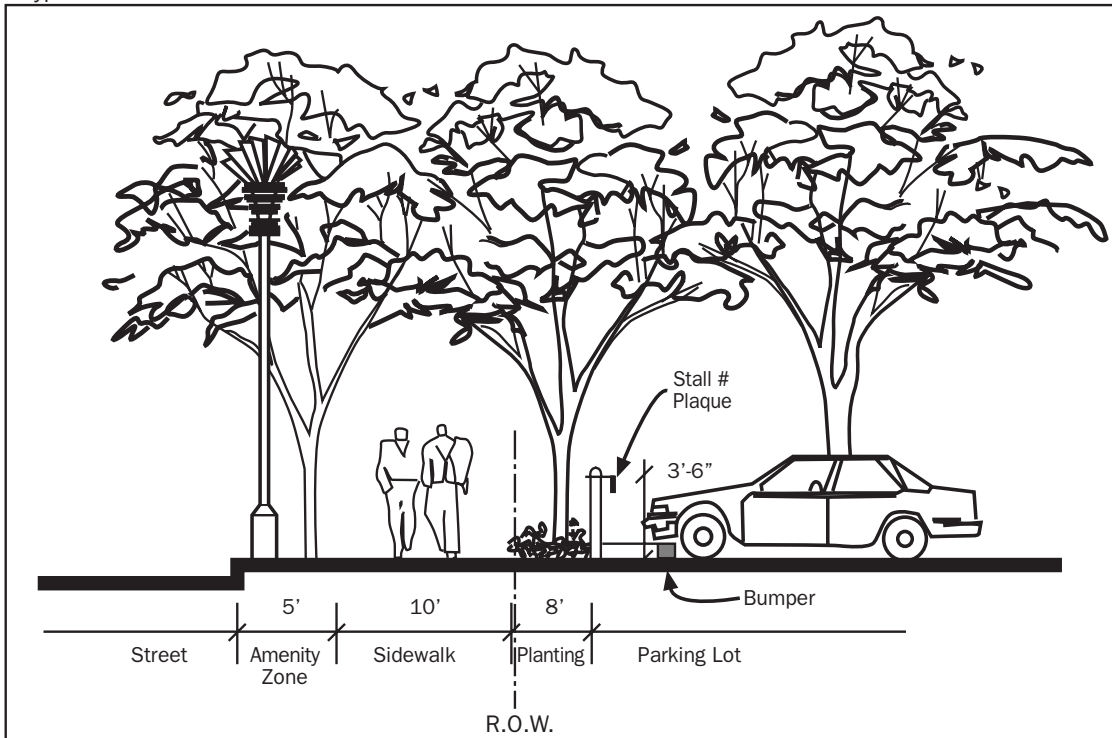


## DS3. DOWNTOWN LAS VEGAS TRANSPORTATION AND PARKING STANDARDS

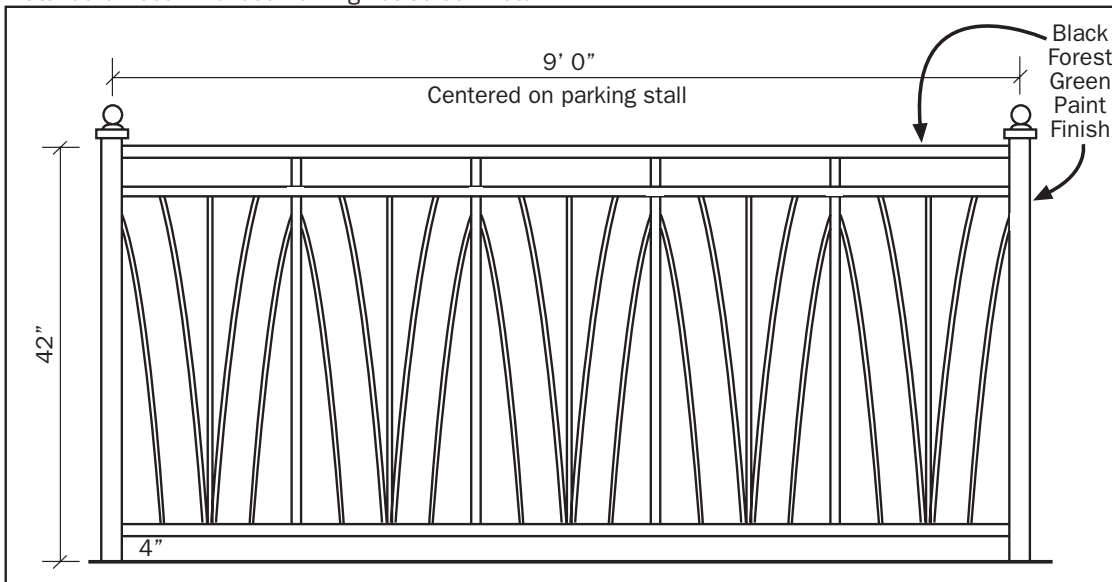
- 1** These are the circulation and parking standards for all of Downtown Las Vegas:
- a. Transportation systems shall be integrated to provide efficient and effective access, parking, and multi-modal opportunities to reduce congestion and pollution.
  - b. Any on-street parking shall not count in meeting parking spaces as required. Limited, on-street parking may be provided as a reserve of short-term parking or for service and delivery purposes.
  - c. All off-street surface parking lots shall be paved and include access aisles and drive-ways. Surface treatments including paving and striping shall be maintained in good condition at all times.
  - d. Whenever a driveway is abandoned, the owner shall remove all driveway pavement and approach, re-install curb and gutter, and landscape the area to match the adjacent landscaping / streetscape.
  - e. Parking areas shall be separated from buildings through surface treatment variations and / or grade separation. Parking spaces that directly abut buildings shall be prohibited.
  - f. Wherever possible, adjacent parcels each requiring access drives shall share a common, centrally located access drive.
  - g. Parking lots facing public streets, major pedestrian walkways, or plazas and open space shall incorporate a minimum eight foot wide landscaped setback with parking lot screen along the entire length of visual exposure (Graphic 9).
  - h. Surface parking lots shall have a minimum of one shade tree for every six spaces in landscaped islands and / or on the perimeter of the lot. A minimum of ten square feet of landscaped surface area must be provided for each parking space, incorporated into islands and / or perimeter landscape treatment, as per Graphic 9.
  - i. Bumpers or tire stops shall be provided in all parking stalls heading into a sidewalk, planting area, street, or alley. They shall be installed at a minimum of two feet from the beginning of the sidewalk, planting area, street, or alley in order to effectively protect walkways, landscaping, and signage (Graphic 9).
  - j. Above grade parking structures shall be set back from streets a minimum of ten feet, except where desirable ground floor uses such as restaurants, retail, and commercial facilities are incorporated into the first floor of the structure facing the street. In this case, the setback is not required and appropriate build-to line designation zones shall apply. The ten foot required setback shall have sufficient landscaping to screen the structure at the pedestrian level, thus providing a visual amenity (Graphic 10).
  - k. All new developments shall provide and install standard Fourth Street style fixtures in place of existing fixtures. Exact specifications, shop drawings, and standard suppliers can be obtained from the City of Las Vegas Engineering Design Superintendent, Department of Public Works, 229-6272.



Typical Street Section



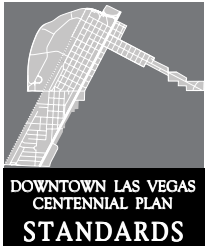
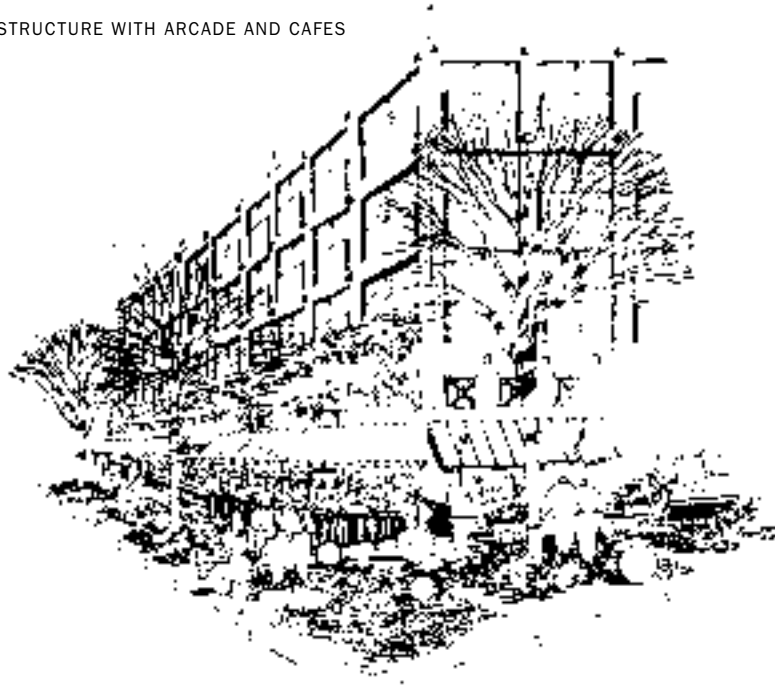
Standard Recommended Parking Lot Screen Detail





- l. Exterior walls of all parking structures shall be designed as part of the architectural form of the main building or surrounding buildings. (The intent is to minimize a garage appearance through designs that conform to the accompanying development's general architectural features.)
- m. Bus drop-off areas shall be provided in close proximity to the entrances of newly constructed hotels. Parking for buses shall be accommodated off-site or in a remote, non-obtrusive location so as not to interfere with normal traffic flow or pedestrian activity to and from the hotel site.
- n. As appropriate with new development, bus turn-outs should be acquired by the City to accommodate bus stops outside the existing travel lanes.
- o. The provisions of Title 19A.10: Parking, Loading and Traffic Standards of the City of Las Vegas Zoning Code, as adopted March 1997, and amended thereafter, shall apply.

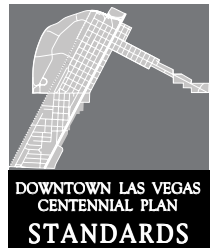
GRAPHIC 10: PARKING STRUCTURE WITH ARCADE AND CAFES



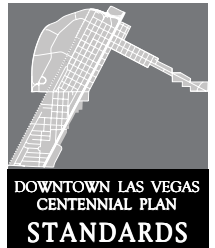


**1** Streets are the primary paths along which people move. They serve as the dominant organizing feature around which buildings, infrastructure improvements, and open spaces are organized. They should never be characterized simply as traffic channels. The idea of a street must extend beyond the right-of-way, property line, or building edge. Streets are inherently organized on a hierarchical system of movement and interest, and their role must be identified in the urban context in terms of image, character, and in functional requirements of linking uses.

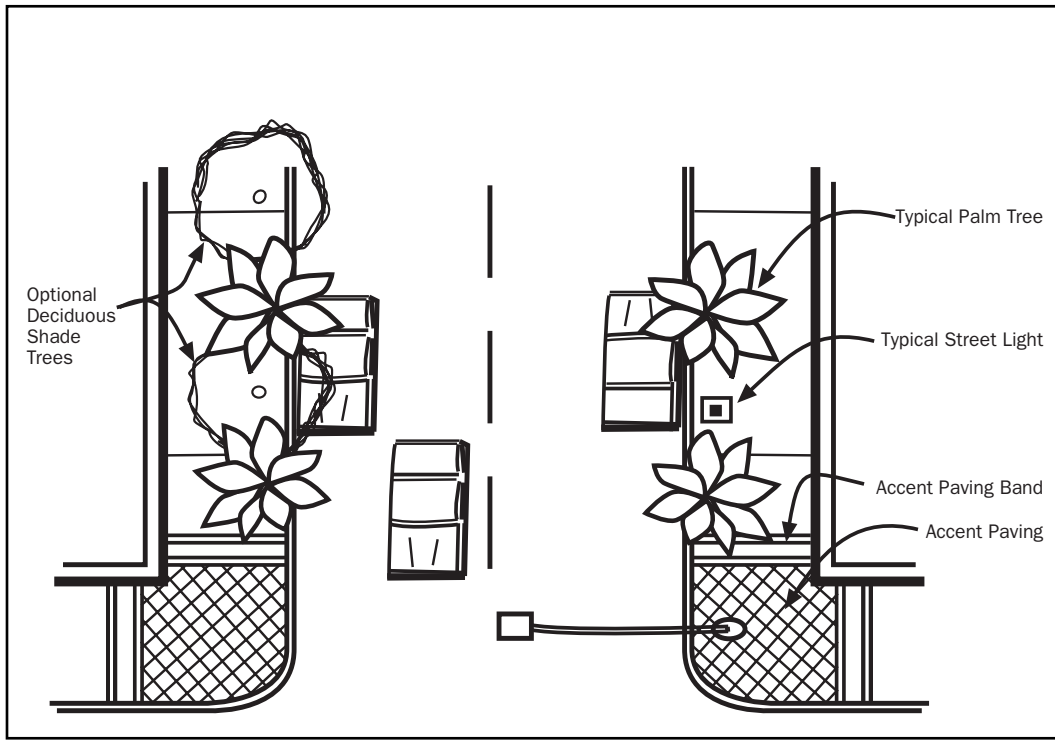
- 2** These are the streetscape design standards for all of Downtown Las Vegas:
- a. North-south streets shall be designed thematically with palm trees as the primary landscape element (25 feet or greater in height with 30 foot spacing preferred and a maximum of 35 foot spacing). Shade trees, in single or double rows, may be provided alternately between palm trees to provide shade and pedestrian comfort (Graphic 11).
  - b. East-west streets shall be designed thematically with shade trees as the primary landscape element (36 inch box trees spaced at 15 to 20 feet preferred to provide an immediate shade canopy, 24 inch box trees at 30 feet maximum spacing required). When shade trees are provided in lieu of a required arcade or other architectural shade device, a double row of 36 inch box (48 inch box preferred) or greater trees placed 15 feet on center is required (Graphic 12).
  - c. Supporting landscape and streetscape improvements shall be incorporated into the streetscape design including decorative paving, lighting, and other landscape elements such as freestanding planters, planter boxes and hanging baskets. Paving must be relatively flat, slip resistant, and of a design that does not create a tripping hazard but adds visual interest and unique imagery. The surface of the sidewalk should appear as a consistent pattern. At the corners, the sidewalk must ramp down to provide for handicapped access. The recent City improvements along the Fourth Street Corridor shall serve as the minimum anticipated streetscape treatment and level of amenity.
  - d. Street lights and decorative pedestrian lights shall be incorporated into any future streetscape improvement. Overhead lights must provide the ambient light necessary for safety and traffic operations, and must meet City of Las Vegas Department of Public Works standards. (See specification in Item "f" below.) Pedestrian lights should be placed in a clear geometric pattern with regular spacing to add a sense of order to the street. The design of the lights and the pattern for their placement are part of the overall design for Downtown Las Vegas. The lights reinforce the hierarchical street system; therefore, lights associated with individual development projects are prohibited from being located in the right-of-way.



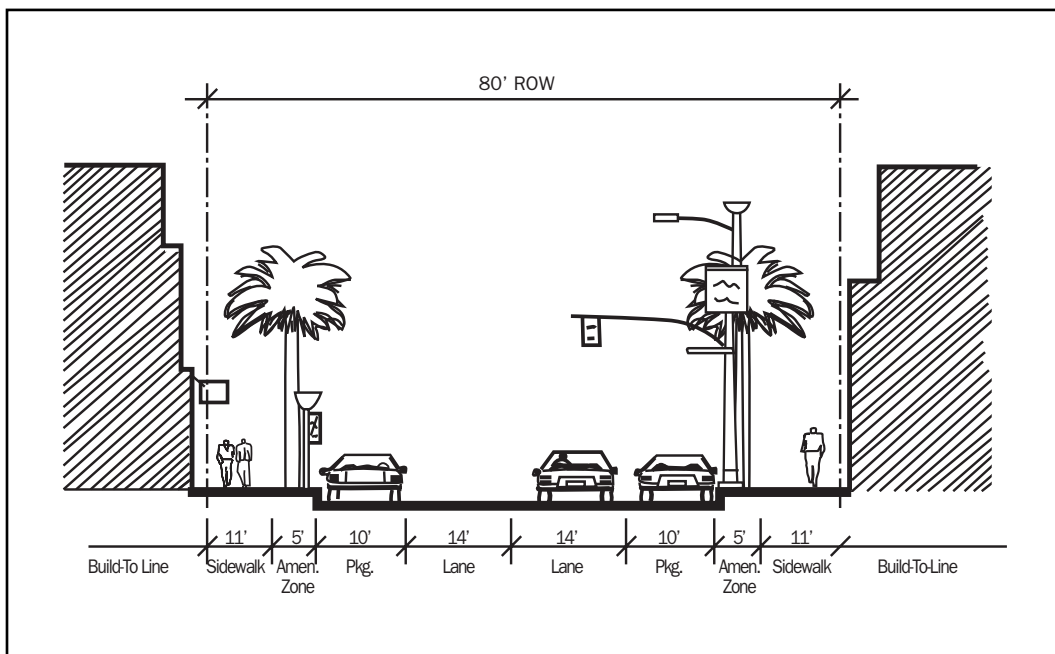
- e. All new streetscapes, sidewalks, and streetscape elements shall conform to match those installed within the Fourth Street Corridor and Las Vegas Boulevard Beautification Projects during 1996 to 1999.
- f. All new developments shall provide and install standard Fourth Street style fixtures in place of existing fixtures. (Exact specifications, shop drawings, and standard suppliers can be obtained from the City of Las Vegas Engineering Design Superintendent, Department of Public Works, 229-6272.)
- g. Landscaping and plant materials shall be as specified in section DS4, Graphic 11 and Graphic 12. (Exact specifications, shop drawings, and standard suppliers can be obtained from the City of Las Vegas Engineering Design Superintendent, Department of Public Works, 229-6272.)



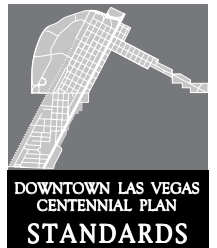
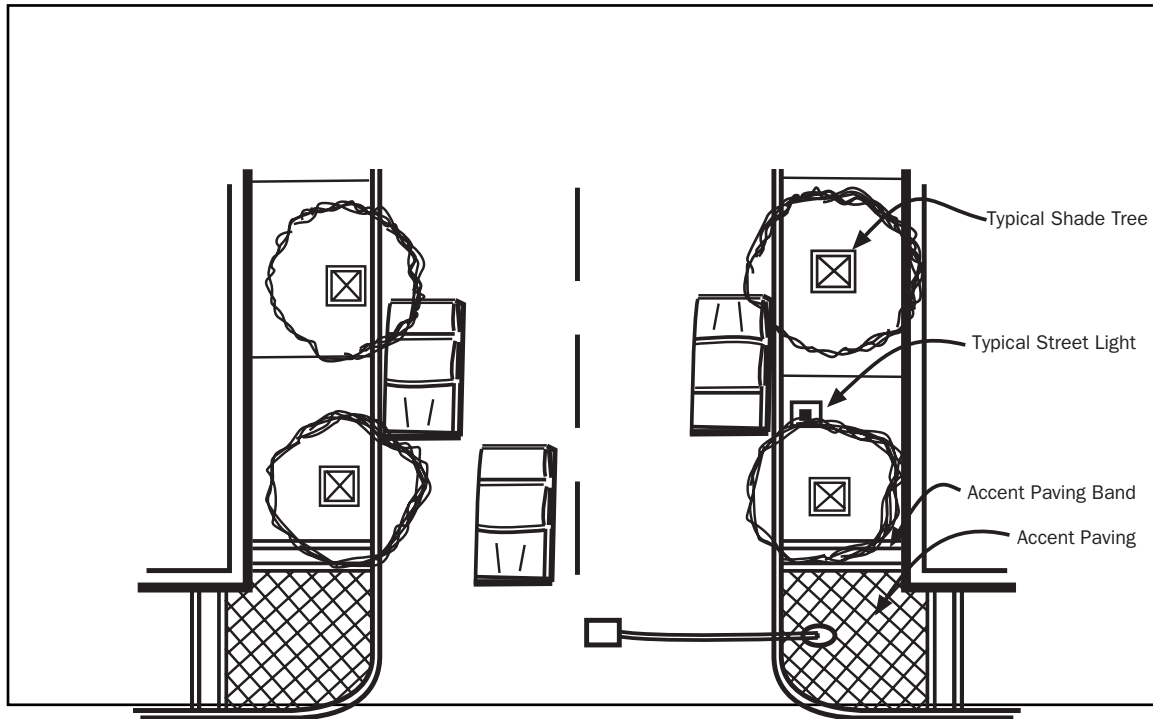
Plan View



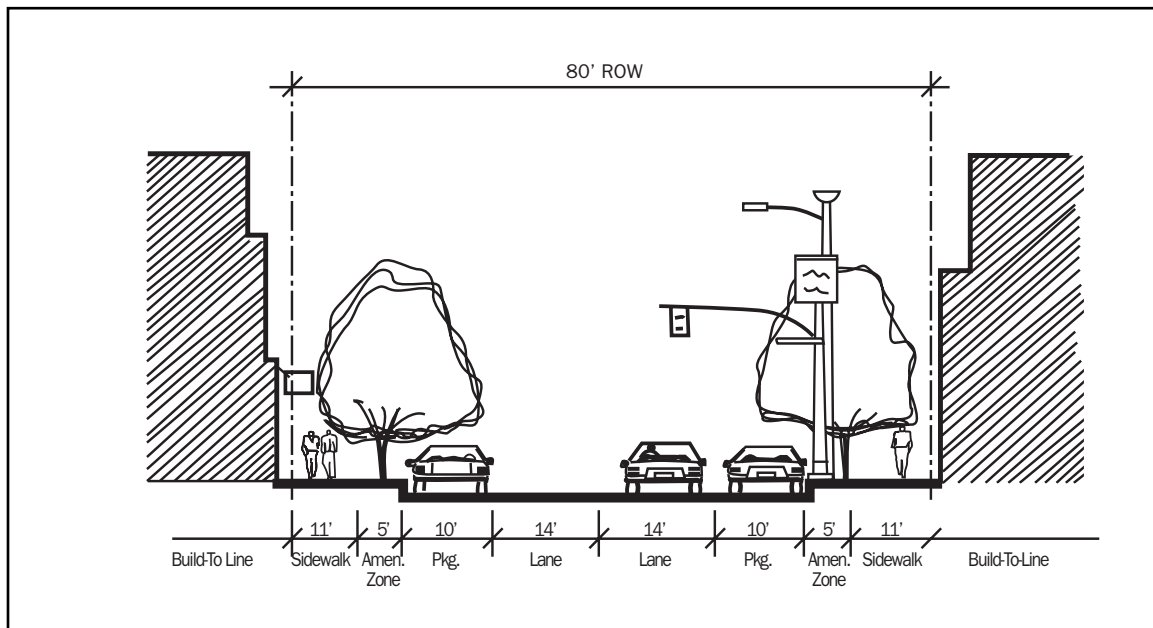
Street Section



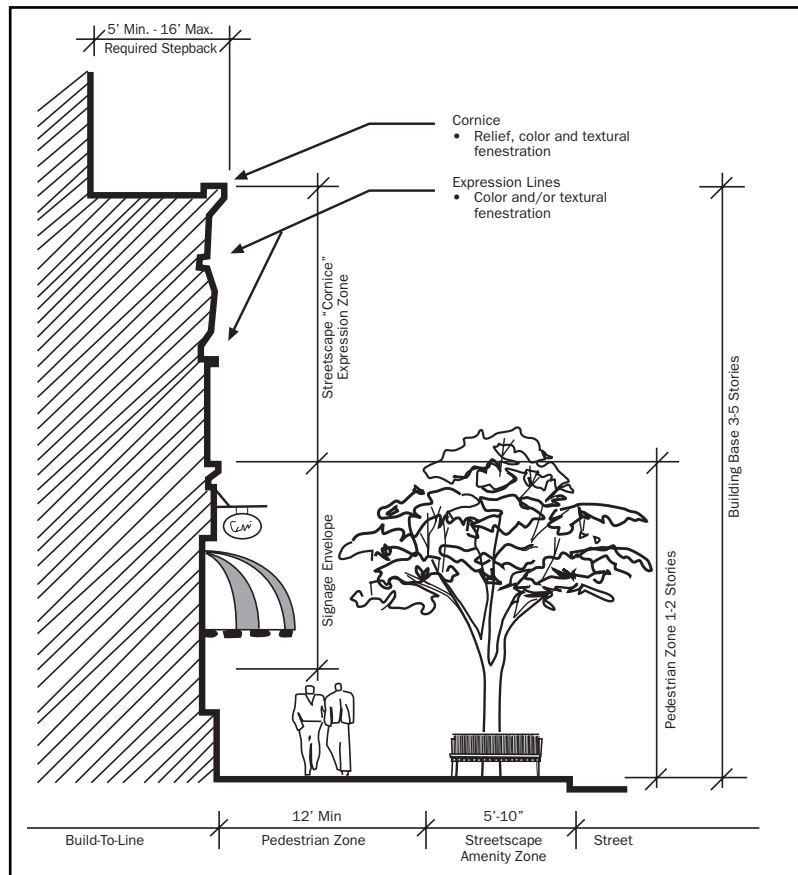
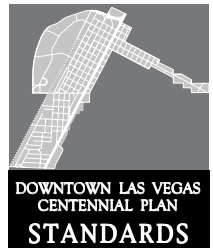
Plan View



Street Section

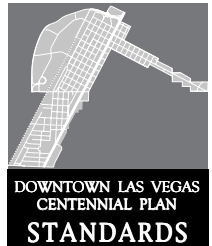


- 1** These are the architectural design standards for all of Downtown Las Vegas:
- Seventy percent (70%) of the building streetwall shall align along the edge of the sidewalk. If immediately adjacent existing buildings are setback, then seventy percent (70%) of the building streetwall may align within the median setback range of existing buildings likely to remain within the same block and street frontage as determined by staff. Setback requirements may be waived by City Council with clear and compelling evidence for buildings that enhance the pedestrian environment through the provision of other pedestrian or civic amenities such as: plazas, courtyards, landscape features, public entries, shade devices, cafes, etc. (Graphic 13).
  - Roofs and rooftops shall be designed with consideration to views from above. No highly reflective materials or contrasting colors shall be used. All mechanical equipment shall be screened from street level and surrounding building views. All obtrusive features shall be minimized.
  - An articulated roof line and / or an articulated cornice shall be designed as a major feature at or near the top of all streetwalls and walls visible from any right-of-way (Graphic 13).

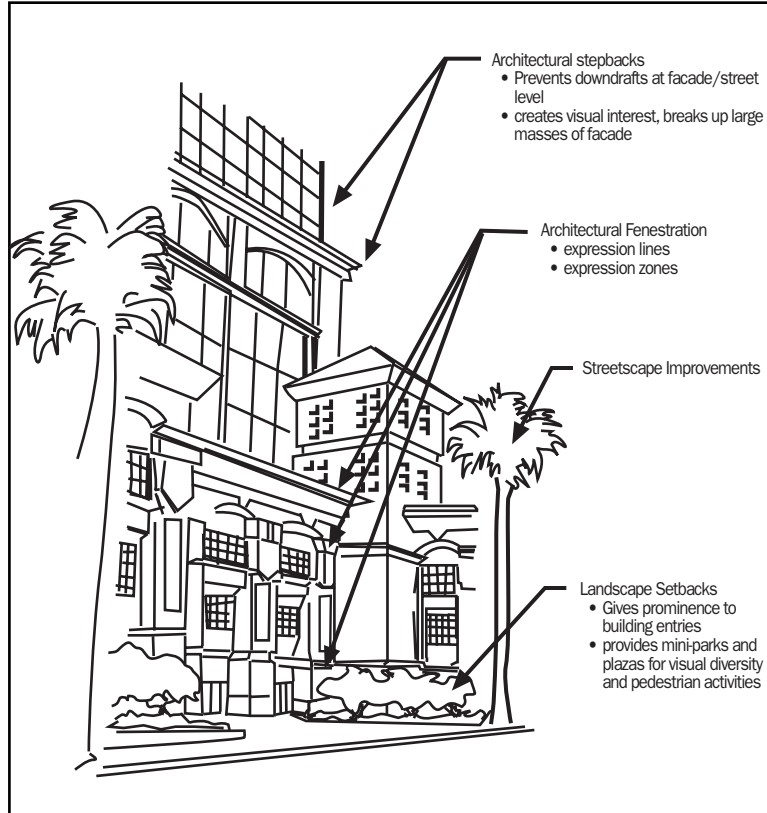


GRAPHIC 13: BUILDING FACADE AT STREET LEVEL

- d. Extended, blank, expressionless walls at the street level shall be prohibited. The use of expression lines and expression zones (that is, areas and features of the facade that incorporate changes in material, color, and / or relief) shall be required in the pedestrian zone to create visually interesting facades (Graphic 14).
- e. The use of arcades, awnings, canopies, and other shade devices is strongly encouraged.
- f. The incorporation of misting systems at the pedestrian level is also strongly encouraged to complement the objectives of this plan.
- g. The main entry of the building from the street shall be appropriately articulated in the architectural design of the building. This shall be accomplished through change in materials, colors, and / or the amount of detailing around the entry; having the entry slightly recessed or protruding from the primary building line; and / or through the use of canopies or awnings, etc. (Graphic 14).
- h. Exterior materials of all buildings shall contribute to a cohesive physical environment, convey a sense of permanence and belonging, and shall be of natural, native materials and/or colors.
- i. Setbacks on streetwalls facing public streets and plazas shall be required to reduce the effects of wind traveling downward along the building face, add visual interest, and promote light penetration at the street level. Setbacks from property lines / sidewalk edges are required at a minimum of 4 stories, 11 stories, and 18 stories in building height. Each setback must be at least five feet in depth, but not greater than 16 feet (Graphic 14).



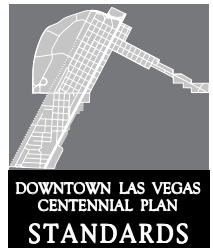
GRAPHIC 14: BUILDING MASSING AND SETBACKS



- j. Service areas (including storage, special equipment, maintenance, and loading areas) shall be screened with landscaping and / or architectural elements from pedestrian or street view. Architectural screening shall be constructed of the same materials and finishes as the primary building.
- k. All service and loading areas shall be positioned so service vehicles will not disrupt traffic flow to or from the site or within parking lots. No service, storage, maintenance, or loading area may extend into a setback area.

## DS6. DOWNTOWN LAS VEGAS SIGNAGE DESIGN STANDARDS

**1** The design, installation, and maintenance, of all signs shall be in full conformance with current Title 19A.14, Sign Standards, as revised and amended, unless subsequent, more restrictive standards are promulgated and amended hereto.



## DS7. DOWNTOWN LAS VEGAS LANDSCAPE DESIGN STANDARDS

**1** Landscaping and planting materials shall be installed per, and in full conformance with, the Las Vegas Urban Design Standards manual section on Landscape, Wall & Buffer Standards and Title 19A as amended, except where City staff and City Council determine that landscaped setbacks are less desirable than enhancement of the pedestrian environment. In such cases, the required landscape setbacks may be waived by City Council with clear and compelling evidence, or landscaped areas relocated on site as determined by staff. When buildings are required to be placed on the property line or edge of sidewalk, the landscape buffer requirement shall be waived for those portions of the parcel where the building meets the sidewalk. These standards complement Title 19A, the City of Las Vegas Zoning Code, and more specifically, Chapter 19A.12: Landscape and Buffer Standards.

**1** The following Office Core Design Standards apply only to the Office Core District. Those standards that are more restrictive than the standards for all other districts appear in bold below. The purpose and intent of establishing Office Core design standards is to give overall direction to future public and private improvements throughout the district.



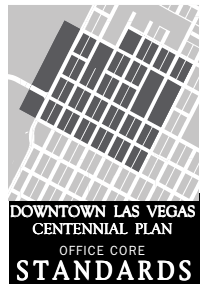
### OC1. OFFICE CORE: SITE PLANNING STANDARDS

- 1** These are the site planning standards for the Office Core District:
- a. **LAND USES:** The following land uses are encouraged within the Office Core District: Office, Office Related, Office Support, Retail, General Commercial, Service Commercial, Restaurants, Cafes, Mixed-Use Developments, Entertainment, and High-Density Residential. Other allowable uses pursuant to Title 19A, City of Las Vegas Zoning Code, as revised, and the City of Las Vegas General Plan, are permissable.
  - b. **BUILDING HEIGHT:** All development proposals for new buildings and structures within the Office Core District shall conform to a minimum building height of two stories unless waived by the City Council for demonstrated economic hardship.
  - c. **FLOOR AREA RATIO:** All development proposals for new buildings and structures within the Office Core District shall conform to a minimum Floor Area Ratio (FAR) requirements, which shall be 1.5 or greater, unless waived by the City Council for demonstrated economic hardship.
  - d. **LOT COVERAGE:** Within the Office Core District, lot coverage may range up to a maximum of 100 percent. No minimum lot coverage has been established.



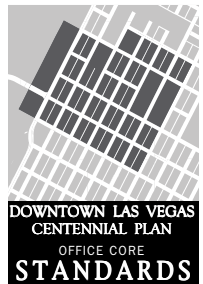
**2** All of the following site planning standards for the Office Core District also pertain to Downtown Las Vegas as a whole:

- a. Plan all structures to conceal service areas and storage areas from public street view. Incorporate these uses into the main building whenever possible.
- b. Orient and screen all auto-related facilities (working bays, storage, etc.) from public view. When parking lots face major public streets, incorporate ornamental screen and landscaping. (see Graphic 9.)
- c. Buildings shall orient their public entrances toward major street frontages as much as possible, particularly on major pedestrian streets.
- d. Utilities and power service lines shall be located underground for all new development. To the extent possible, relocation of utilities from alleys (above ground) to streets (below ground) shall be undertaken with all new substantial development activities.
- e. When alleyways are abandoned and building envelopes expanded to create larger development parcels and eliminate negative character of existing alleys, the developer shall relocate utilities and design sidewalks to meet the approved design. If the alley is not abandoned then it shall conform to the standard Alleyway Treatment (Graphic 8).

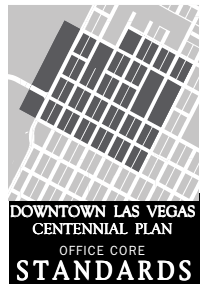


## OC2. OFFICE CORE: TRANSPORTATION AND PARKING STANDARDS

- 1** All of the following transportation and parking standards for the Office Core District also pertain to Downtown Las Vegas as a whole:
- a. Any on-street parking shall not count in meeting parking spaces as required. Limited, on-street parking may be provided as a reserve of short-term parking or for service and delivery purposes.
  - b. All off-street surface lots shall be paved and include access aisles and driveways. Surface treatments including paving and striping shall be maintained in good condition at all times.
  - c. Whenever a driveway is abandoned, the owner shall remove all driveway pavement and approach, re-install curb and gutter, and landscape the area to match the adjacent landscaping / streetscape.
  - d. Parking areas shall be separated from buildings through surface treatment variations, such as landscape buffers, and / or by grade separation. Parking spaces that directly abut buildings shall be prohibited.
  - e. Wherever possible, adjacent parcels each requiring access drives should share a common, centrally located access drive.
  - f. Parking lots facing public streets, major pedestrian walkways, or plazas and open space shall incorporate a minimum eight foot wide landscaped setback with parking lot screen along the entire length of visual exposure (Graphic 9).
  - g. Surface parking lots shall have a minimum of one shade tree for every six spaces in landscaped islands and / or on the perimeter of the lot. A minimum of ten square feet of landscaped surface area must be provided for each parking space, incorporated into islands and / or perimeter landscape treatment, with 15-gallon bush per 10 square feet of area.
  - h. Above grade parking structures shall be set back from streets a minimum of ten feet, except where desirable ground floor uses such as restaurants, retail, and commercial facilities are incorporated into the first floor of the structure facing the street. In this case, the setback is not required and appropriate build-to line designation zones shall apply. The ten foot required setback shall have sufficient landscaping to screen the structure at the pedestrian level, thus providing a visual amenity.
  - i. Bumpers or tire stops shall be provided in all parking stalls heading into a sidewalk, planting area, street, or alley. They shall be installed at a minimum of two feet from the beginning of the sidewalk, planting area, street, or alley in order to effectively protect walkways, landscaping and signage.
  - j. Exterior walls of all parking structures shall be designed as part of the architectural form of the main building or surrounding buildings. The intent is to minimize a garage appearance through designs that conform to the accompanying development's general architectural features.

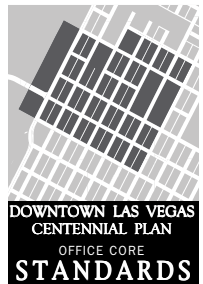


- k. Bus drop-off areas shall be provided in close proximity to the entrances of newly constructed hotels. Parking for buses shall be accommodated off-site or in a remote, non-obtrusive location so as not to interfere with normal traffic flow or pedestrian activity to and from the hotel site.
- l. The provisions of Title 19A.10: Parking, Loading and Traffic Standards of the City of Las Vegas Zoning Code, as adopted March 1997, and amended thereafter, shall apply. CCDC may suggest alternate standards which may apply to the Office Core in the future.



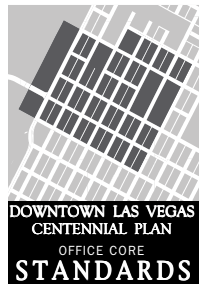
**1** All of the following streetscape design standards for the Office Core District also pertain to Downtown Las Vegas as a whole:

- a. North-south streets shall be designed thematically with palm trees as the primary landscape element (25 feet or greater in height spaced at 30 feet preferred, however, 35 feet maximum spacing). Shade trees, in single or double rows, may be provided alternately between palm trees to provide shade and pedestrian comfort (Graphic 11).
- b. East-west streets shall be designed thematically with shade trees as the primary landscape element (36 inch box trees spaced at 15 to 20 feet preferred to provide an immediate shade canopy, 24 inch box trees at 30 feet maximum spacing required). When shade trees are provided in lieu of an arcade or other architectural shade device, a double row of 36 inch box trees placed 15 feet on center is encouraged (Graphic 12).
- c. Supporting landscape and streetscape improvements shall be incorporated into the streetscape design, including decorative paving, lighting, and other landscape elements such as freestanding planters, planter boxes and hanging baskets. Paving must be relatively flat, slip resistant, and of a design that does not create a tripping hazard but adds visual interest and unique imagery. The surface of the sidewalk should appear as a consistent pattern. At the corners, the sidewalk must ramp down to provide for handicapped access.
- d. Street lights and decorative pedestrian lights shall be incorporated into any future streetscape improvement. Overhead lights must provide the ambient light necessary for safety and traffic operations, and it must meet City of Las Vegas Department of Public Works standards. (See specification in Item “e” below.) Pedestrian lights should be placed in a clear geometric pattern with regular spacing to add a sense of order to the street. The design of the lights and the pattern for their placement are part of the overall design for Downtown Las Vegas. The lights reinforce the hierarchial street system. Therefore, lights associated with individual development projects are prohibited from being located in the right-of-way.
- e. All new streetscapes, sidewalks, and streetscape elements shall conform to exactly match those installed within the Fourth Street Corridor and Las Vegas Boulevard Beautification Projects during 1996 to 1999. (Exact specifications, shop drawings, and standard suppliers can be obtained from the City of Las Vegas Engineering Design Superintendent, Department of Public Works, 229-6272.)

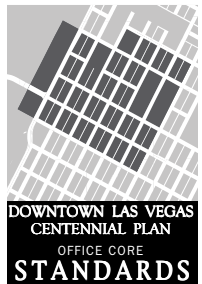


**1** All of the following architectural design standards for the Office Core District, except the first two standards, also pertain to Downtown Las Vegas as a whole:

- a. All new or renovated buildings and structures shall provide ground floor retail (or other pedestrian supported activities) along seventy percent (70%) of the building streetwall, for a minimum depth of twenty five feet (25') from the outside of the streetwall.
- b. Seventy percent (70%) of walls at the ground level on primary pedestrian routes and streets shall be composed of transparent glazing. Reflective glass at the pedestrian level is strictly prohibited. Glass on the second floor and above shall not be greater than twenty two percent (22%) reflectivity so as to not reflect light and solar heat on other buildings, streets and sidewalks.
- c. Seventy percent (70%) of the building streetwall shall align along the edge of the sidewalk. If adjacent buildings are setback, the seventy percent (70%) of the building streetwall shall align within the median setback range of existing buildings likely to remain within the same block and street frontage. Setback requirements may be waived by staff or City Council for buildings that substantially enhance the pedestrian environment through the provision of other pedestrian or civic amenities such as: plazas, courtyards, landscape features, public entries, shade devices, cafes, etc.
- d. Roofs and rooftops shall be designed with consideration to views from above. No highly reflective materials or contrasting colors shall be used. All mechanical equipment shall be screened from street level and surrounding building views. All obtrusive features shall be minimized.
- e. An articulated roof line and / or an articulated cornice shall be designed as a major feature at or near the top of all streetwalls and walls visible from any right-of-way. Buildings with flat roofs shall incorporate a parapet wall around the entire perimeter of the building to shield the roof from street view and hide mechanical equipment, etc.
- f. Extended, blank, expressionless walls at the street level shall be prohibited. The use of expression lines and expression zones (that is, areas and features of the facade that incorporate changes in material, color, and / or relief) shall be required in the pedestrian zone to create visually interesting facades (Graphic 13).
- g. The use of arcades, awnings, canopies, and other shade devices is strongly encouraged.
- h. The incorporation of misting systems at the pedestrian level is also strongly encouraged.
- i. The main entry of the building from the street shall be appropriately articulated in the architectural design of the building. This shall be accomplished through change in materials, colors and / or the amount of detailing around the entry; having the entry slightly recessed or protruding from the primary building line; and / or through the use of canopies or awnings, etc.



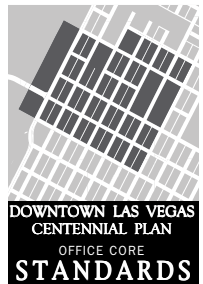
- j. Exterior materials of all buildings shall contribute to a cohesive physical environment, convey a sense of permanence and belonging, and preferably be of natural, native materials.
- k. Setbacks on streetwalls facing public streets and plazas shall be required (to reduce the effects of wind traveling downward along the building face) to add visual interest and promote light penetration at the street level. Setbacks are required at a minimum of four stories, 11 stories, and 18 stories in building height. Each setback must be at least five feet in width (Graphic 14).
- l. Service areas (including storage, special equipment, maintenance, and loading areas) shall be screened with landscaping and / or architectural elements from pedestrian or street view. Architectural screening shall be constructed of the same materials and finishes as the primary building.
- m. All service and loading areas shall be positioned so service vehicles will not disrupt traffic flow to or from the site or within parking lots. No service, storage, maintenance or loading area may extend into a setback area.



## OC5. OFFICE CORE: SIGNAGE DESIGN STANDARDS

**1** Within the Office Core District, the design, installation, and maintenance of all signs, shall be in full conformance with Title 19A.14, Sign Standards, as revised and amended, unless subsequent more restrictive standards are promulgated and amended hereto.

**1** Within the Office Core District, landscaping and planting materials shall be installed per, and be in full conformance with, the Las Vegas Urban Design Standards manual section on Landscape, Wall & Buffer Standards. These standards complement Title 19A, the City of Las Vegas Zoning Code, and more specifically, Chapter 19A.12: Landscape and Buffer Standards. Where specific landscape standards or elements are included within the District Plans contained herein, they shall supersede these and other existing standards.

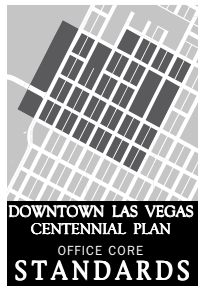
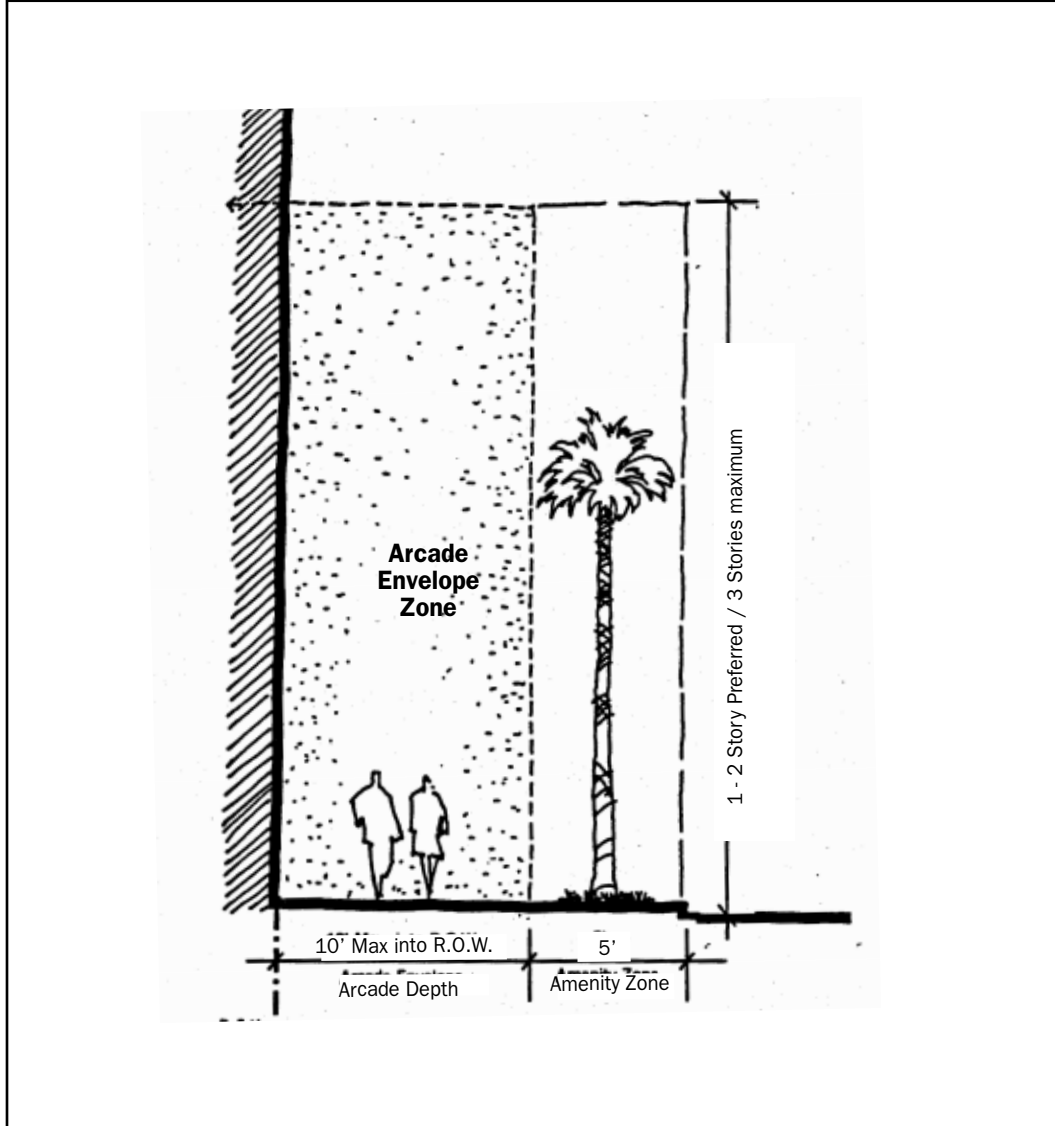


## OC7. OFFICE CORE: ARCADE DESIGN STANDARDS

**1** These are the minimum arcade design standards (Graphic 15) for the Office Core District when arcades are provided:

- a. All arcades must meet the fundamental goal of providing a sun screen, shade, or shelter for the pedestrian on the sidewalk. This goal must be accomplished to accommodate the angles of the sun from 11:00 a.m. to 3:00 p.m., May 15 to September 15 of each year.
- b. Signage advertising businesses and their addresses may occur on the arcade either under the protective cover or on the face of the arcade fronting the street. No sign occupying space beneath the protective cover may exceed eight square feet in area. No sign applied to the face of the arcade fronting a street may exceed 24 square feet in area. Neon and external illumination of signage is acceptable. Large, internally illuminated, back-lit panels are prohibited. Free-standing signs are prohibited.
- c. Design creativity and design excellence are encouraged in the execution of arcades. As part of the commercial heritage of Las Vegas, innovative responses to the design of arcades will contribute to an enduring image, character, and culture of Las Vegas. Adherence to the guiding principles of streetscape continuity, district image, pedestrian comfort, and streetwall uniformity are very important.

Typical Section



Typical Plan View

